

# **County of Wilts 1916 Plan of Bridges on Main & District roads over the Wilts & Berks Canal**

## **Introduction**

At the Wiltshire & Swindon History Centre in Chippenham, there is a document from Wiltshire County Council (ref: F4/150/42) setting out its plans for culverting bridges over what was then the defunct Wilts & Berks and North Wilts canals. The County Surveyor concentrated on the main roads and local roads, ignoring the minor country lanes.

The document is printed on an early photo-stat paper that's translucent and, unfortunately, the text and lines tend to be fading or even blurred.

The County's classification of 'main roads' used its own numbering system. Although the classification into A, B and C roads had been proposed in 1913, the First World War delayed its implementation until 1923. No doubt the war influenced Wiltshire County Council's decision to reduce long-term maintenance costs by replacing the bridges with culverts, usually with 24" glazed socketted pipes jointed in cement mortar.

When the canals closed, there were protests from the rural areas because these waterways were important for land drainage, and hence there needed to be culverts to allow the water to flow as necessary.

For each bridge, the document has one page showing the current plan and elevation, plus the proposed plan and elevation; in addition, there are up to two pages listing materials and works with planned costs. The lists of materials, works and costs are usually over-written with actual figures. They follow a fairly uniform pattern suggesting that the works were carried out over a relatively short period; there was considerable price inflation during the war, but the actual costs were not that much different from what was planned.

Swindon Borough was administered separately from the rural county of Wiltshire, and so decisions on its bridges were outside the scope of this document; nevertheless, some of the bridges listed here are within the current boundary of Swindon because the town grew geographically over the decades into areas such as Moredon, Wroughton and South Marston.

Overleaf, one of the bridges' material costs are listed as a typical example. Then follow the plans and elevations bridge-by-bridge in the order in which they appear in the document. In many cases, the plans and elevations are the only images we have of the shapes and dimensions of the original structures.

BRIDGE ON ROAD BETWEEN BROAD TOWN & WOOTTON  
BASSETT

Approximate Estimated cost of work to be done in demolition and  
Re-construction including making up Roadway and the works  
Contingent thereto.

Yds	ft	in				£.	s.	d.
353 ✓	-	-	Sup	Removing and depositing metal in road crust for reuse.	6d	8.	6.	6. ✓
55 ✓	-	-	"	Taking down parapet walls and depositing materials for re-use in foundation to road.	1/6	4.	2.	6. ✓
44 ✓	-	-	"	Taking down arch and depositing materials for re-use in foundation to road	1/9	3.	17.	0. ✓
56 ✓	-	-	"	Taking haunch and wing walls and depositing materials for re-use	2/-	5.	12.	0. ✓
185 519			Item	Allow for damming back water during progress of works		<del>5. 0. 0.</del>	<del>2. 10. 0.</del>	
<del>38</del> 42	-	-	lin	In 24" dia glazed socketted pipes jointed in cement mortar	6/-	<del>11. 8. 0.</del>	<del>12. 12. 0.</del>	✓
40 ✓	-	-	Sup	In two 4 1/2" brick rings 2ft 6" long at inlet and outlets of drain including cement concrete benching	1/6	3.	0.	0. ✓
27 ✓	-	-	Cube	In wheeling, depositing, spreading and lightly ramming earth around pipes	1/6	2.	0.	6. ✓
290 ✓	-	-	"	In excavating and depositing materials in reducing level of roadway including consolidation.	2/6	36.	5.	0. ✓
No 4 14				Oak posts cut of 6" x 6" each 7ft long to form fencing including painting 3 oils.	10/-	7.	0.	0. ✓
<del>220</del> x 330			lin	In 2" galvanized steel tubing forming fencing including painting 3 oils.	1/6	<del>16. 0. 0.</del>	<del>24. 15. 0.</del>	
183 ✓	-	-	Sup	In consolidating block stone in foundation to roadway	1/-	9.	3.	0. ✓
<del>40</del> 4	-	-	Sup	In block limestone for foundation	8/-	<del>16. 0. 0.</del>	<del>1. 12. 0.</del>	
183 ✓	-	-	Sup	In forming roadway in limestone to a consolidated thickness of 4"	5/-	45.	15.	0. ✓

x This plan  
has been altered  
since the measure-  
ments were taken

169. 0. 6

✓

Item Allow for making good wastes and verges

Item Allow for watching and lighting works during progress.

Contingencies allow. 5%

✓

169. 0. 6  
£ s d  
~~1. 10. 0.~~ ✓  
~~2. 0. 0.~~ ✓  
2. 10. 0. ✓  
9. 15. 0.

Total. £ 184. 4. 6.

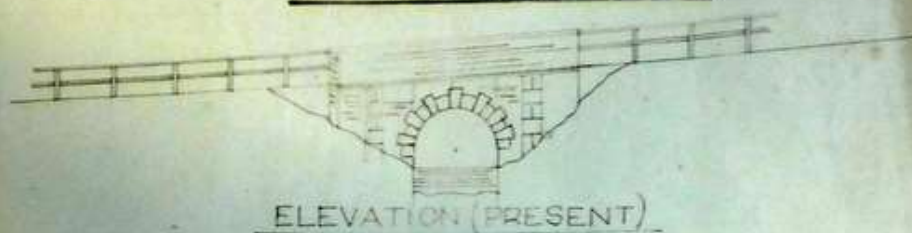
182. 15. 6  
£182. 15. 6

## Bridges listed in order of appearance:

	Black Dog Bridge – main Road No. 13.
	Canal Bridge Road 11, Derry Hill.
	Dance Bridge Main Road No. 2.
	Dauntsey Lock Bridge.
	Melksham Lowbourn Canal bridge Main Road No. 2
	Spa Road Bridge, Melksham. Main Road No. 14.
	Bridge between Broad Town and Wootton Bassett.
	Bridge between Wootton Bassett and Lyneham Main Road No. 2 (Tockenham).
	Charlecutt Bridge, Calne RDC.
	Foxham Lock Bridle bridge, Calne RDC.
	Foxham Bridge.
	Gallows Footbridge, Calne RDC.
	Hazelend Bridge (Studley – Bremhill).
	Stanley Bridge (Chippenham – Bremhill).
	Wick Bridge (Bremhill – Calne).
	Bowden Hill Bridge (Chippenham – Lacock).
	City Road Bridge, Foxham (Christian Malford – Lyneham).
	Rey Bridge (Lacock – Chippenham).
	Studley Bridge (Chippenham – Bremhill).
	Bishop Fowley Bridge (Wootton Bassett – not stated).
	Canal Tunnel, Cricklade (Minety – Cricklade).
	Canal Tunnel, Cricklade (Cricklade – Malmesbury).
	Cross Lanes Bridge (canal line parallel to Purton – Cricklade road).
	Dudge Moor Draw Bridge (Cricklade – Purton).
	Dudgemore No. 2 Draw Bridge (Cricklade – Purton).
	Dudge Moor Bridge No. 3 (brick-built, hump-back, on access to farm from main road to Cricklade).
	Forty Bridge (Chelworth – Cricklade).
	Greenhill Bridge No. 1 (Wootton Bassett – Tockenham).
	Purton Pry Draw Bridge (from Purton Road to farm).
	Tockenham Wick Draw Bridge (Grittenham – Tockenham).
	Wood Shaw Bridle Bridge (brick-built, hump-back to culvert).
	Bassett Bridge (Lydiard Tregoze – Wroughton).
	Moredon Bridge, Highworth RDC.
	Stratton Wharf Bridge, Highworth RDC (Cricklade – Highworth).
	Tockenham Bernard Bridle Bridge (Wootton Bassett to farm).
	Wroughton Bridge (Highworth RDC).
	Bowd's Farm Bridge (Malmesbury RDC) – flat bridge with embanked approaches – flattened.
	Trow Lane Bridge (Malmesbury RDC) – brick-built, hump-backed. Flattened.
	Waite Hill Farm Draw Bridge (Malmesbury RDC), near Dauntsey.



# BLACK DOG BRIDGE MAIN ROAD NO. 13.



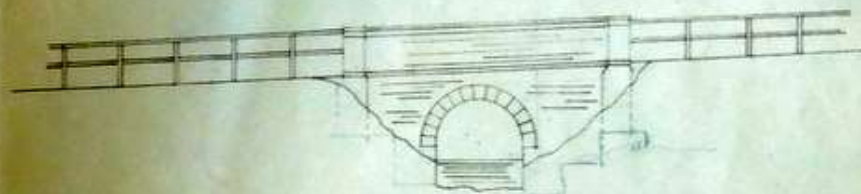
ELEVATION (PRESENT)

FROM CHIPPENHAM

TO CALNE



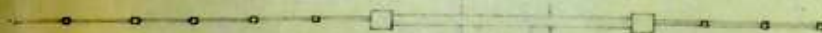
PLAN (PRESENT)



ELEVATION (PROPOSED)

FROM CHIPPENHAM

TO CALNE



PLAN (PROPOSED)

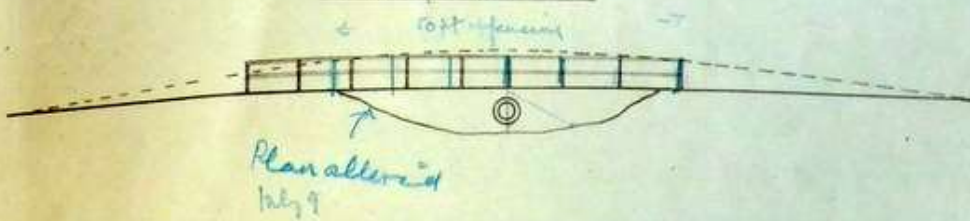
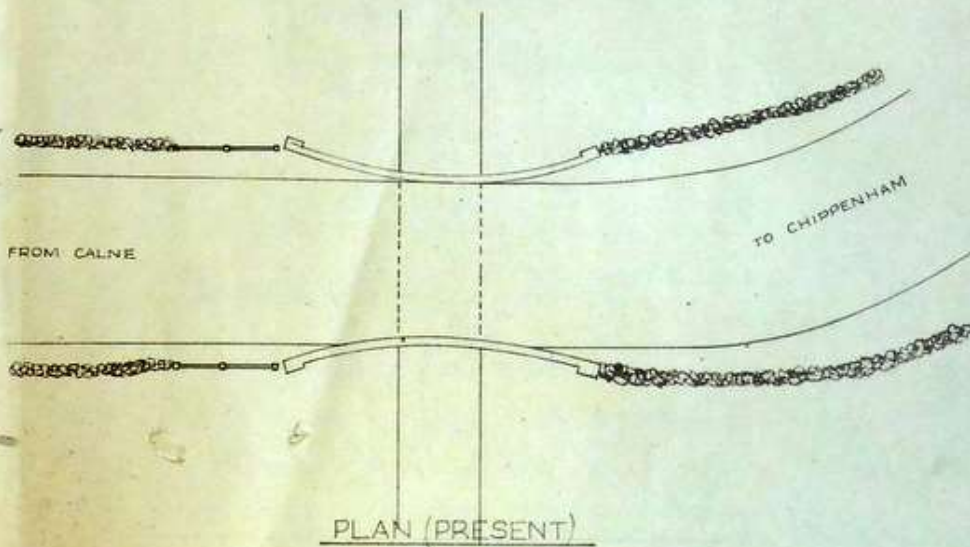
PLAN (PROPOSED)

CANAL BRIDGE

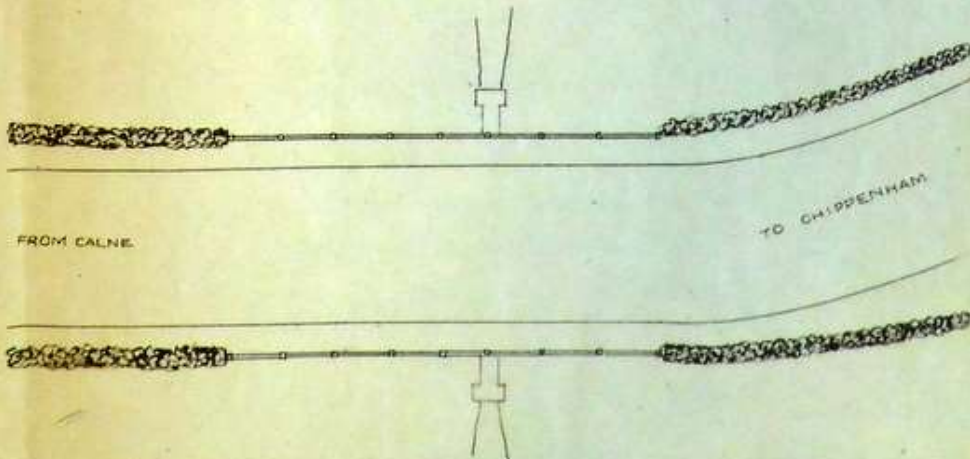
DERRY MILL



ELEVATION (PRESENT)



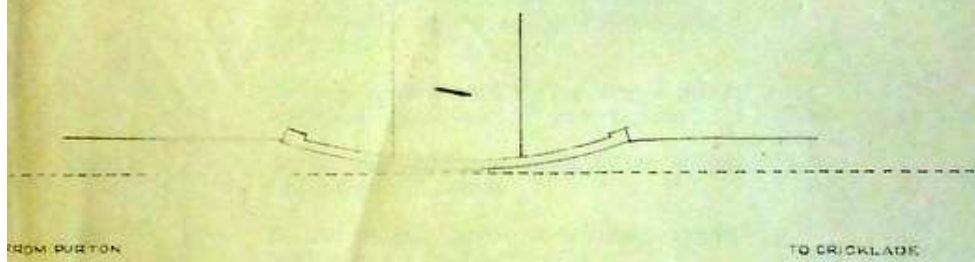
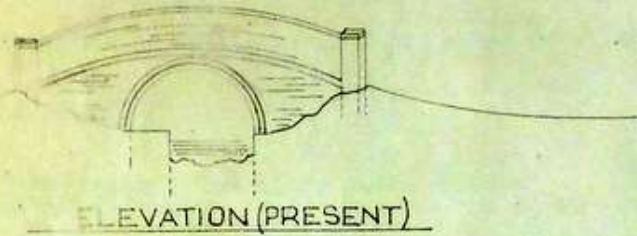
ELEVATION (PROPOSED)



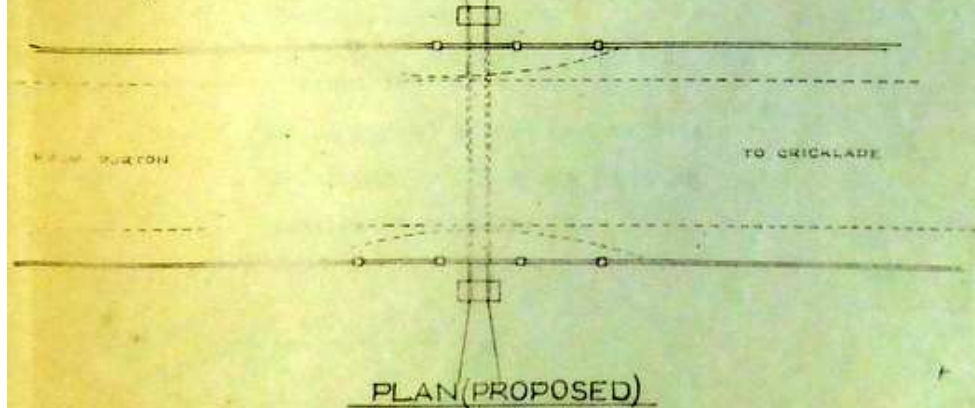
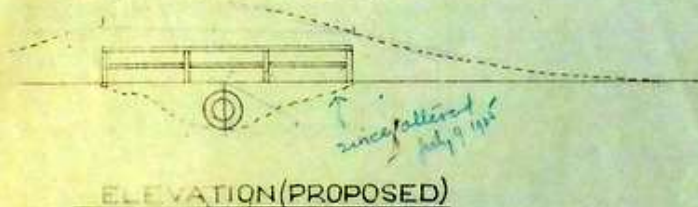
PLAN (PROPOSED)



**DANCE BRIDGE**  
**MAIN ROAD NO 2.**

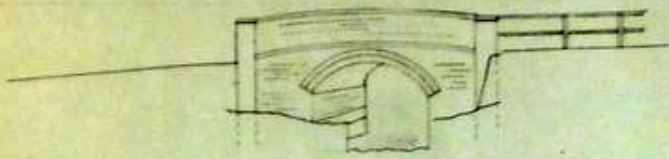


PLAN (PRESENT)

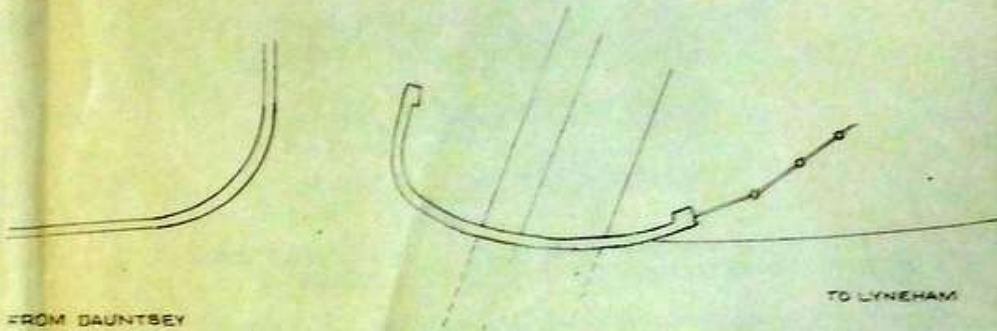


# DAUNTSEY LOCK BRIDGE.

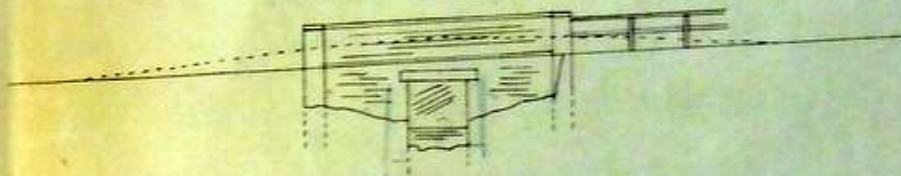
36



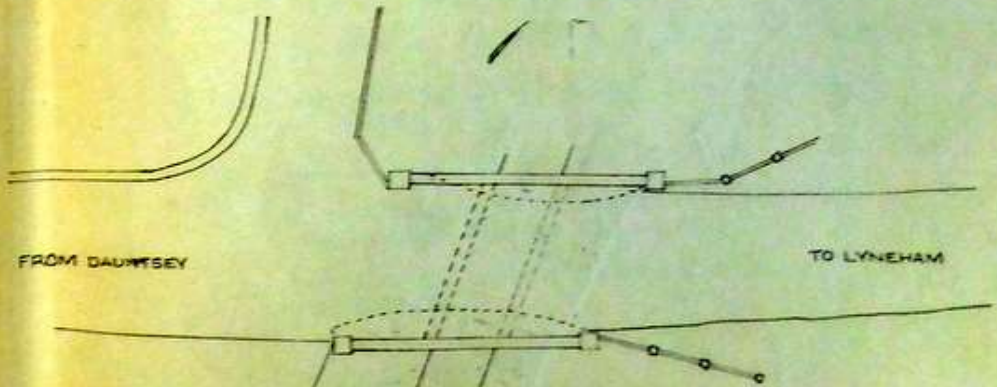
ELEVATION (PRESENT)



PLAN (PRESENT)



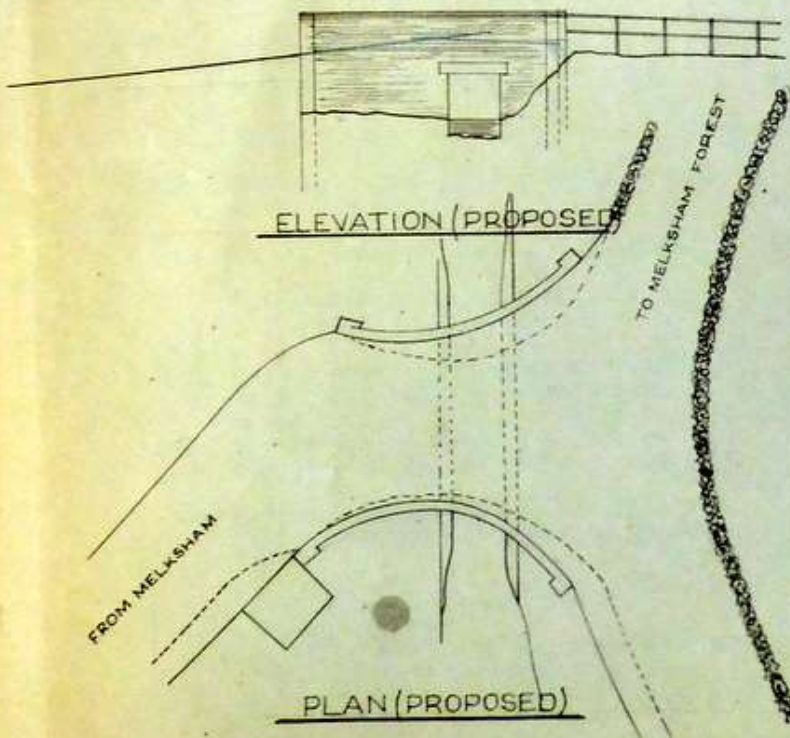
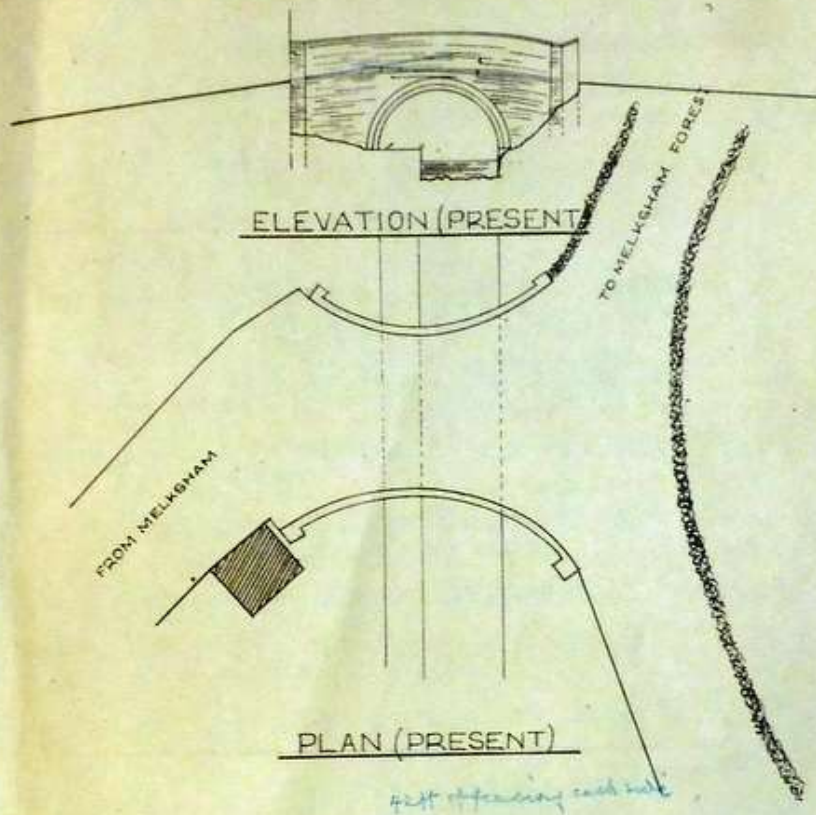
ELEVATION (PROPOSED)



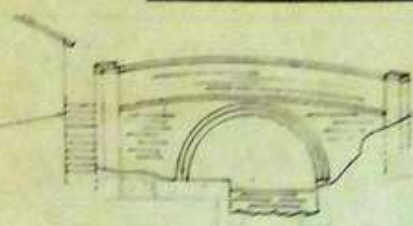
PLAN (PROPOSED)



MELKSHAM LOWBOURNE CANAL BRIDGE.  
MAIN ROAD NO 2



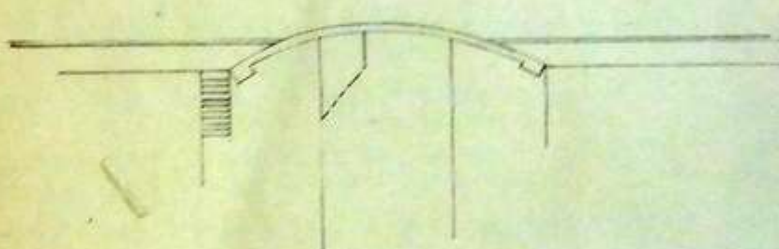
SPA ROAD BRIDGE, MELKSHAM.  
MAIN ROAD NO 14.



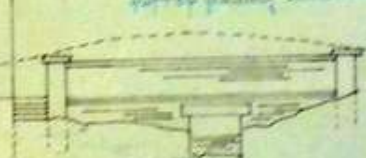
ELEVATION (PRESENT)

FROM MELKSHAM

TO DEVIZES




PLAN (PRESENT)



ELEVATION (PROPOSED)

FROM MELKSHAM

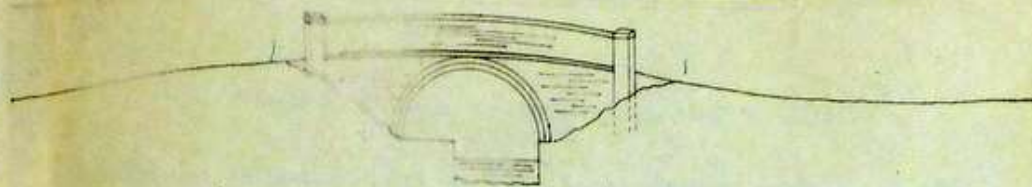
TO DEVIZES



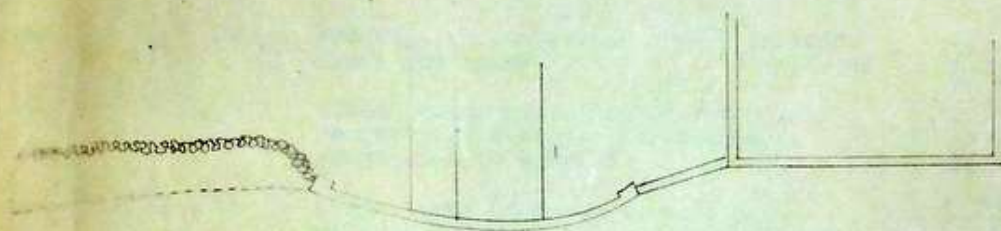
PLAN (PROPOSED)



# BRIDGE BETWEEN BROADTOWN AND WOOTTON BASSETT.

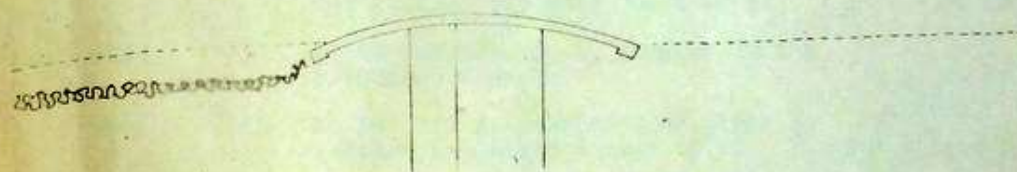


ELEVATION (PRESENT)



FROM BROADTOWN

TO WOOTTON BASSETT

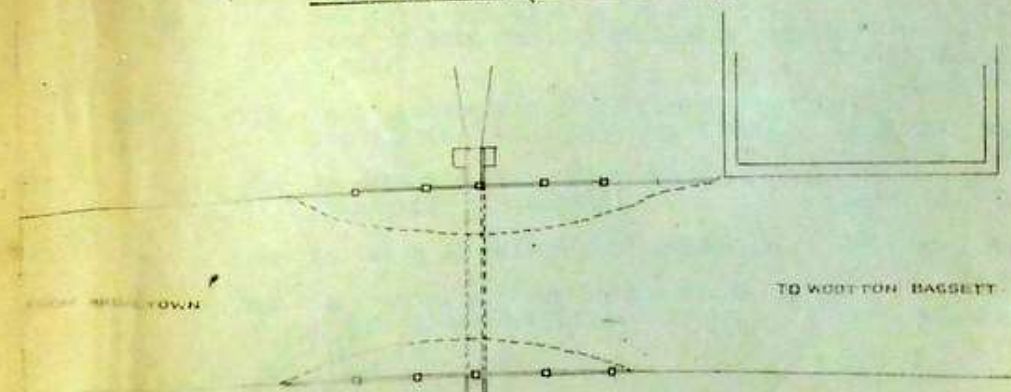


*Stiffening marsh side*

PLAN (PRESENT)



ELEVATION (PROPOSED)



FROM BROADTOWN

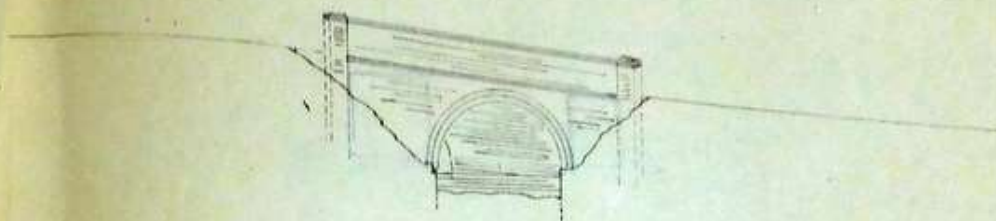
TO WOOTTON BASSETT

PLAN (PROPOSED)

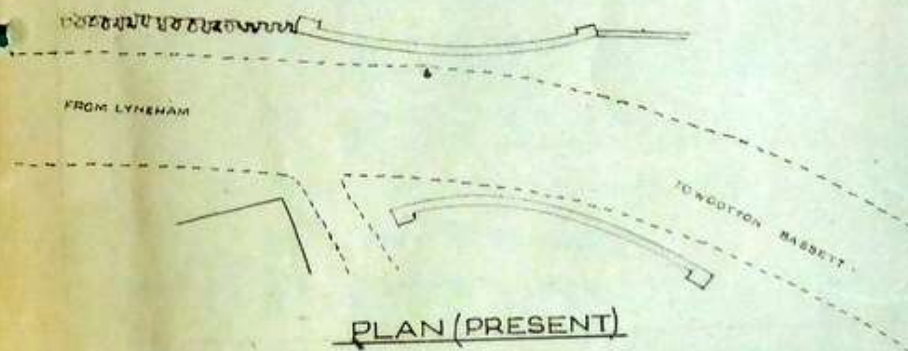


**BRIDGE BETWEEN WOOTTON BASSETT  
AND LYNEHAM.  
MAIN ROAD NO 2.**

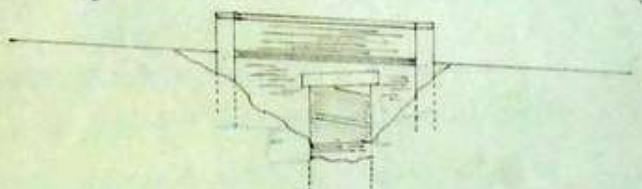
*Doekenhame*



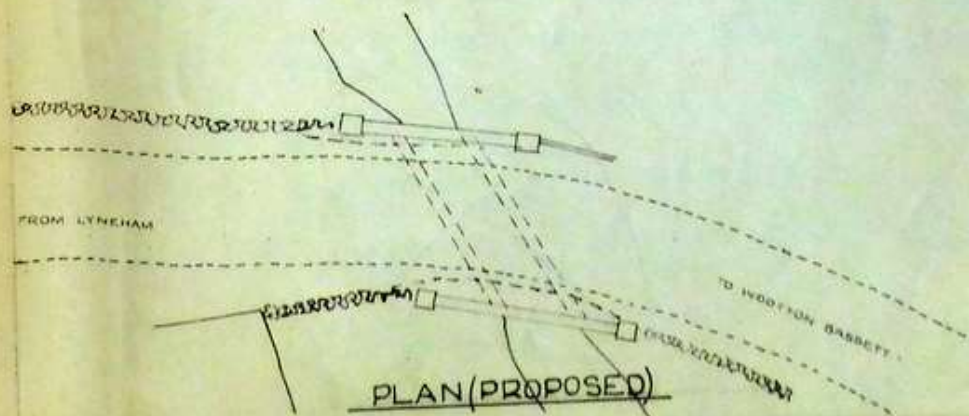
ELEVATION (PRESENT)



PLAN (PRESENT)



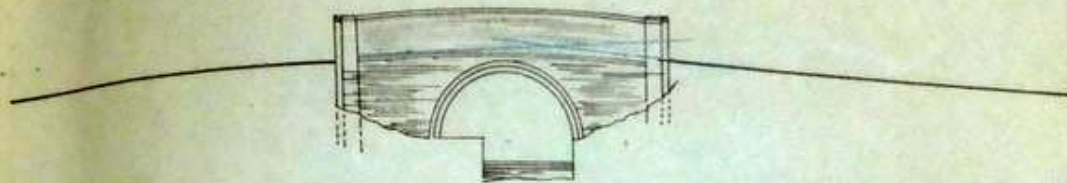
ELEVATION (PROPOSED)



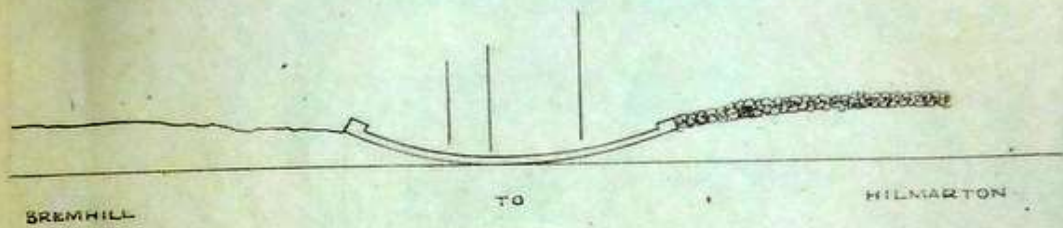
PLAN (PROPOSED)

# CHARLCUTT BRIDGE.

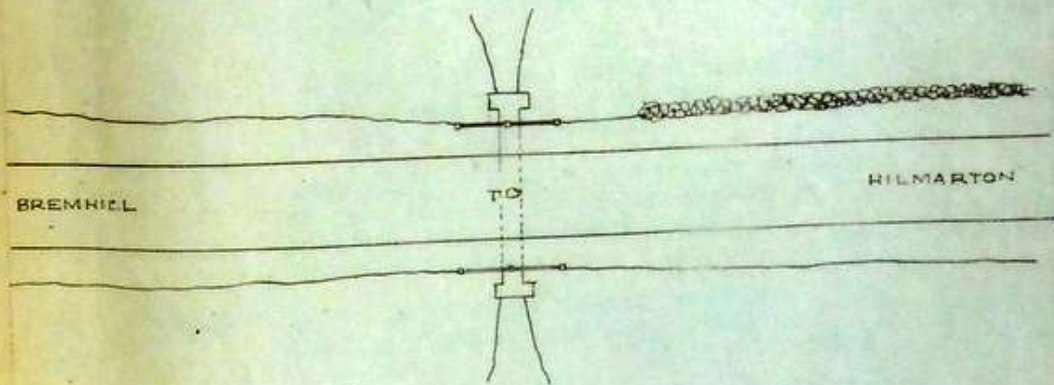
CALNE R.D.C.



ELEVATION (PRESENT)



PLAN (PRESENT)



PLAN (PROPOSED)

*5 ft 6 in of each side*



ELEVATION (PROPOSED)

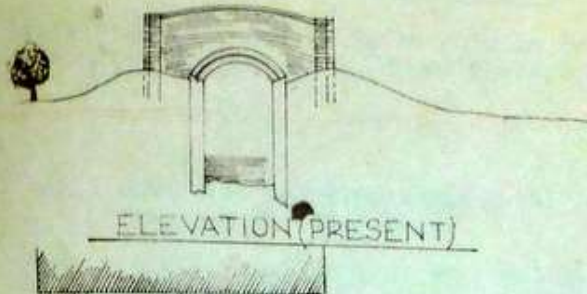
*plan altered  
July 9, 1910*



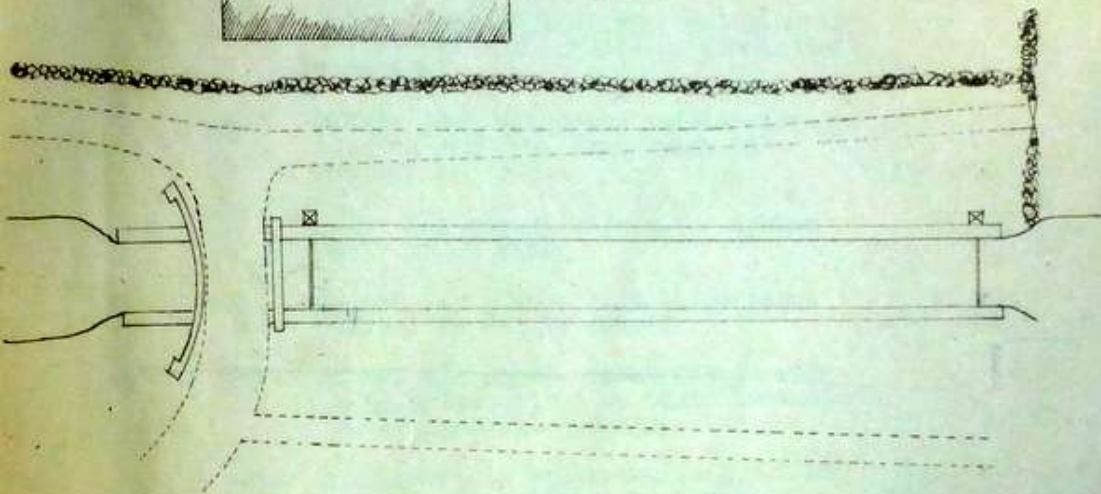
FOXHAM LOCK BRIDLE BRIDGE.

12

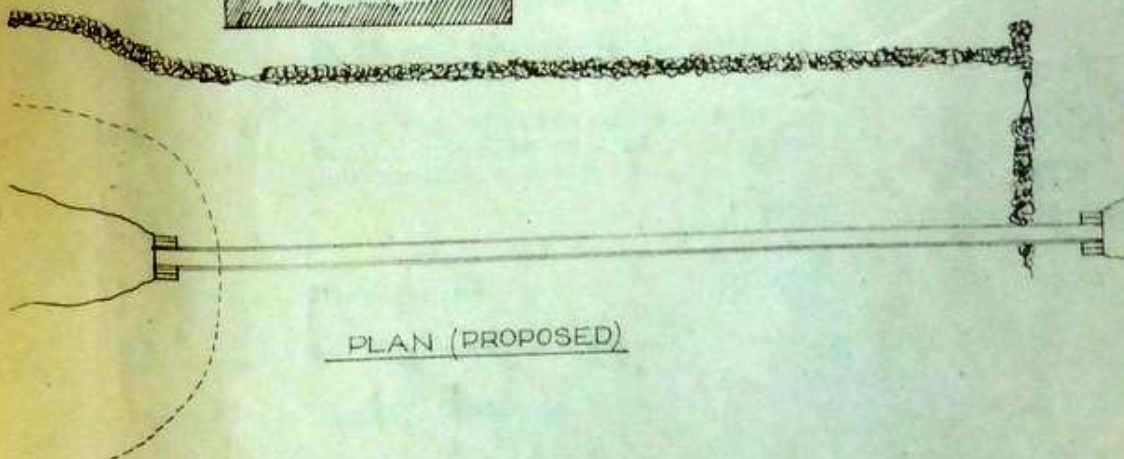
CALNE R.D.C.



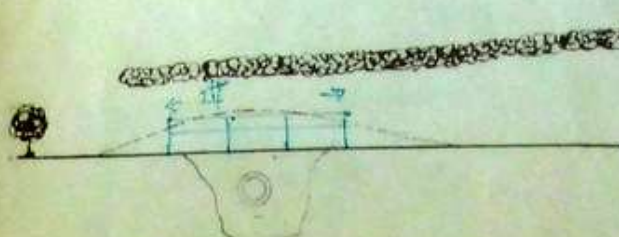
ELEVATION (PRESENT)



PLAN (PRESENT)



PLAN (PROPOSED)

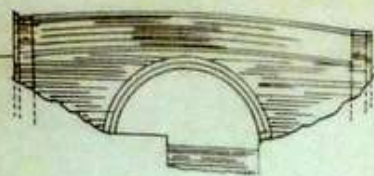


ELEVATION (PROPOSED)

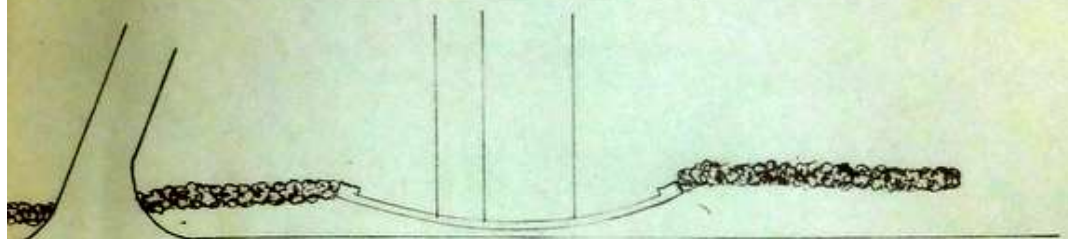


FOXHAM BRIDGE.  
CALNE · R.D.C.

11

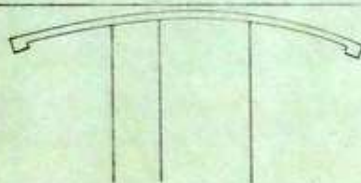


ELEVATION (PRESENT)

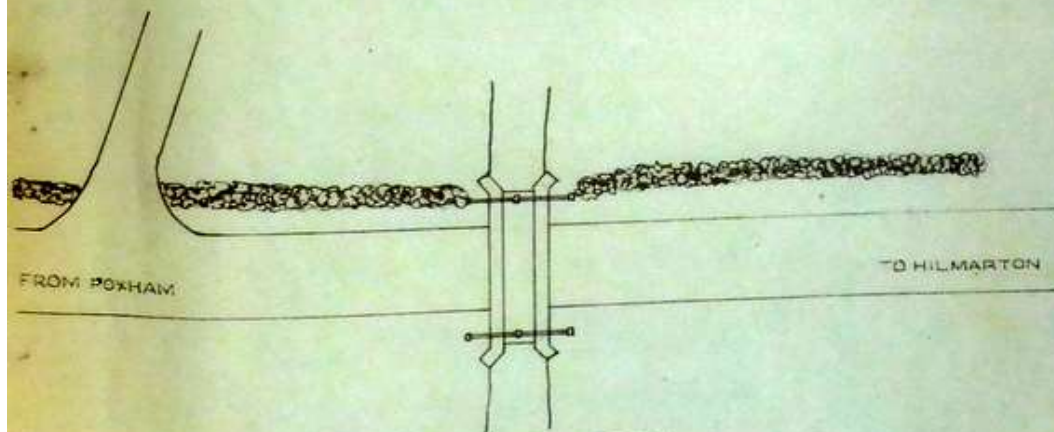


FROM FOXHAM

TO HILMARTON



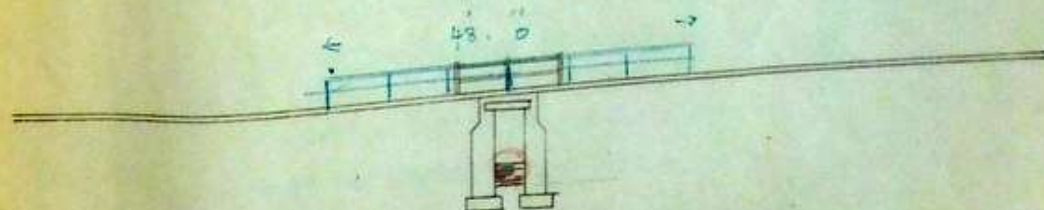
PLAN (PRESENT)



FROM FOXHAM

TO HILMARTON

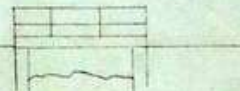
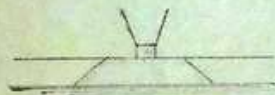
PLAN (PROPOSED)



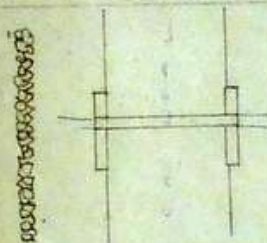
ELEVATION (PROPOSED)

# GALLOWS FOOTBRIDGE.

CALNE · R · D · C ·

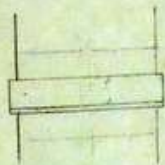


SECTION (PRESENT) ELEVATION (PRESENT)



PLAN (PRESENT)

SECTION (PROPOSED)



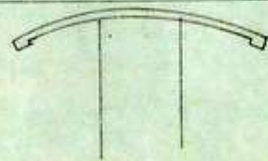
PLAN (PROPOSED)

ELEVATION (PROPOSED)



A diagram of a curved beam, likely a segment of a circular arch or a curved structural member. The beam is shown in profile, curving upwards at both ends. Two vertical lines are drawn through the beam, indicating a cross-section or a specific structural analysis point.

TO BREMHILL



Plan altered  
July 9.

A diagram illustrating the construction of a hyperbola. Two intersecting lines, labeled  $AB$  and  $CD$ , intersect at point  $E$ . A circle is drawn with center  $E$  and radius  $EA$ . The circle intersects the lines  $AB$  and  $CD$  at points  $A$  and  $C$  respectively. The hyperbola is constructed by drawing two branches that pass through points  $A$  and  $C$  and are symmetric with respect to the lines  $AB$  and  $CD$ . The hyperbola is shown as a dashed line.

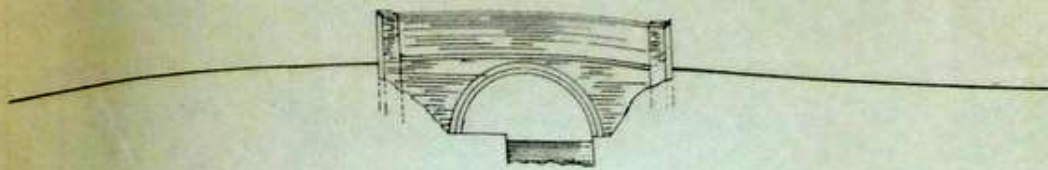
TO BARNHILL.

PLAN (PROPOSED)

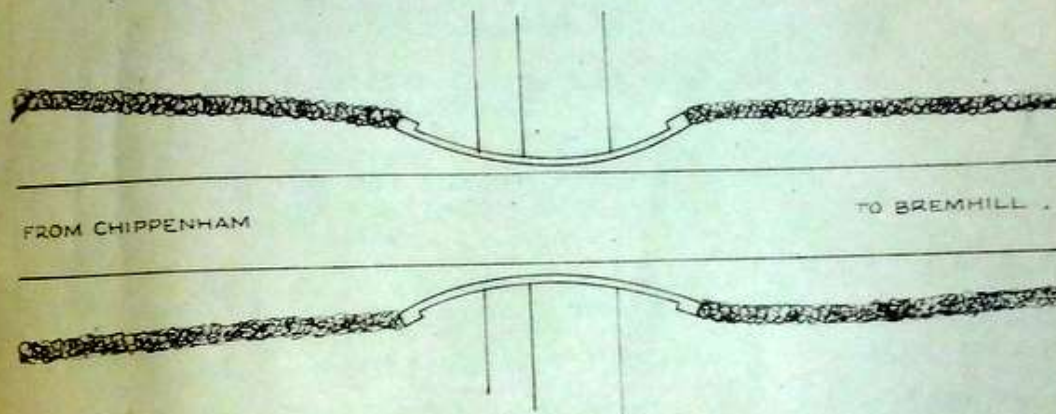


# STANLEY BRIDGE

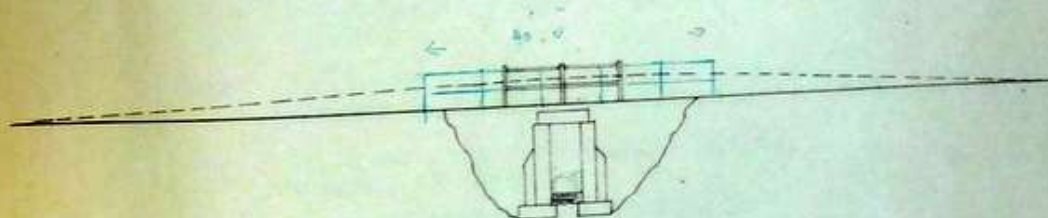
CALNE R.D.C.



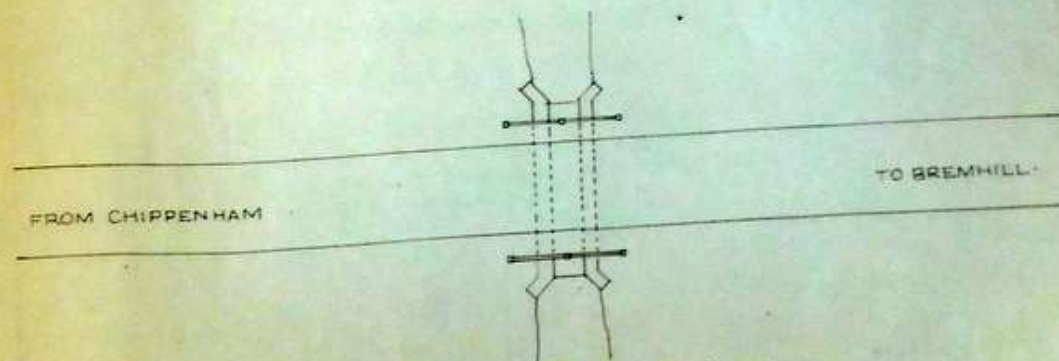
ELEVATION (PRESENT)



PLAN (PRESENT)



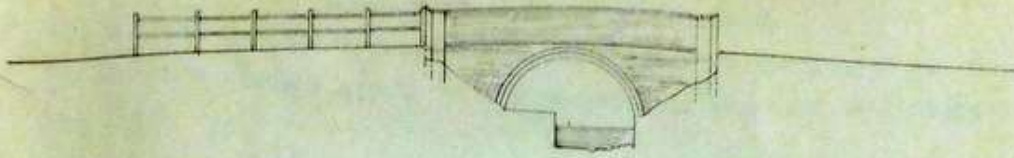
ELEVATION (PROPOSED)



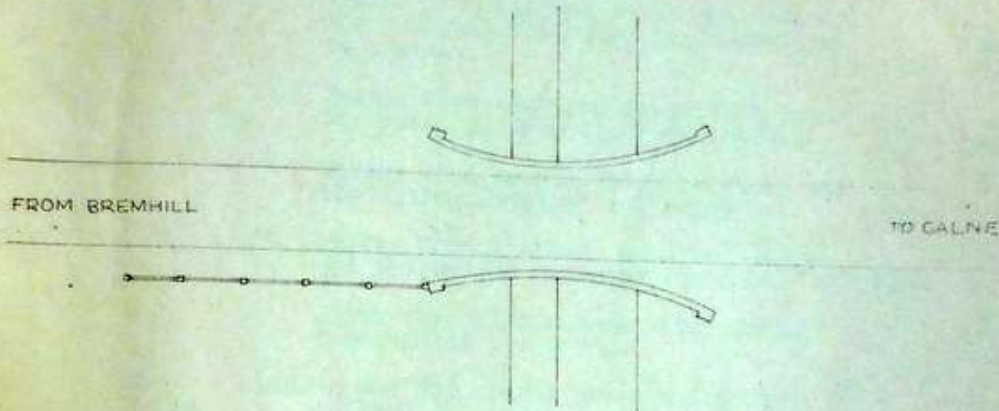
PLAN (PROPOSED)

WICK BRIDGE.  
CALNE R.D.C.

10



ELEVATION (PRESENT)

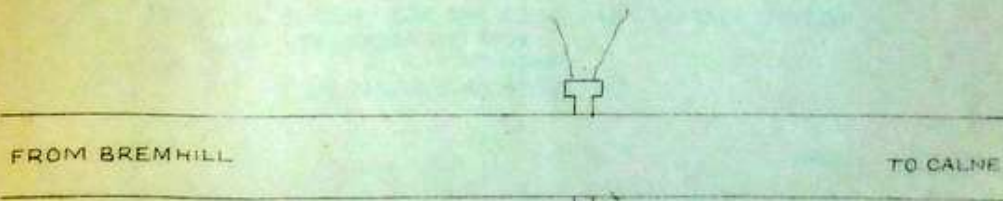


PLAN (PRESENT)



ELEVATION (PROPOSED)

*plan altered  
July 9.11*



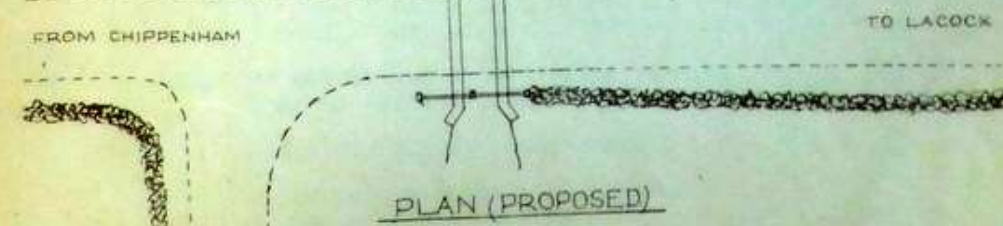
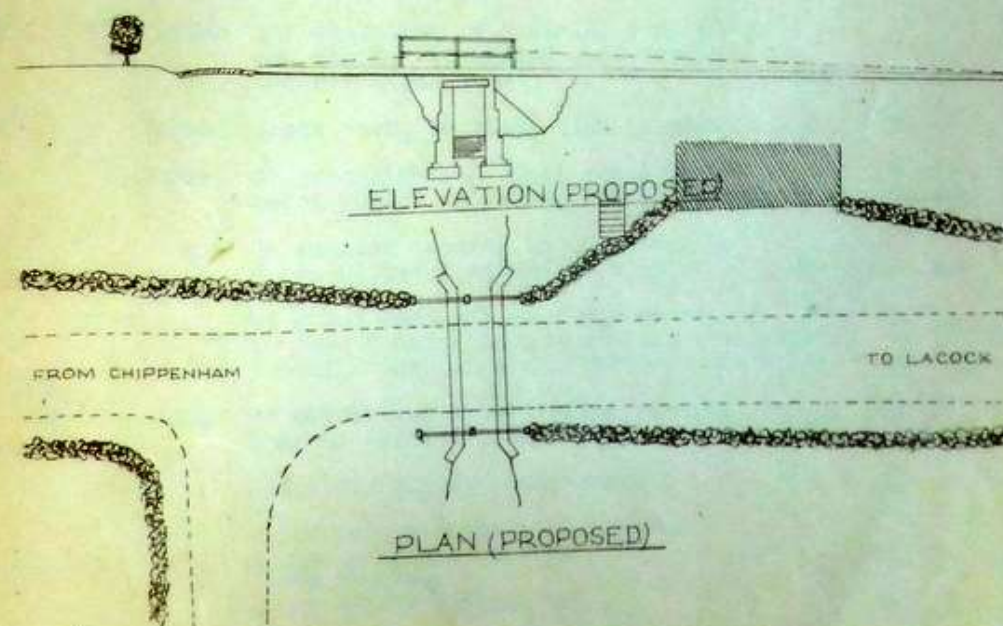
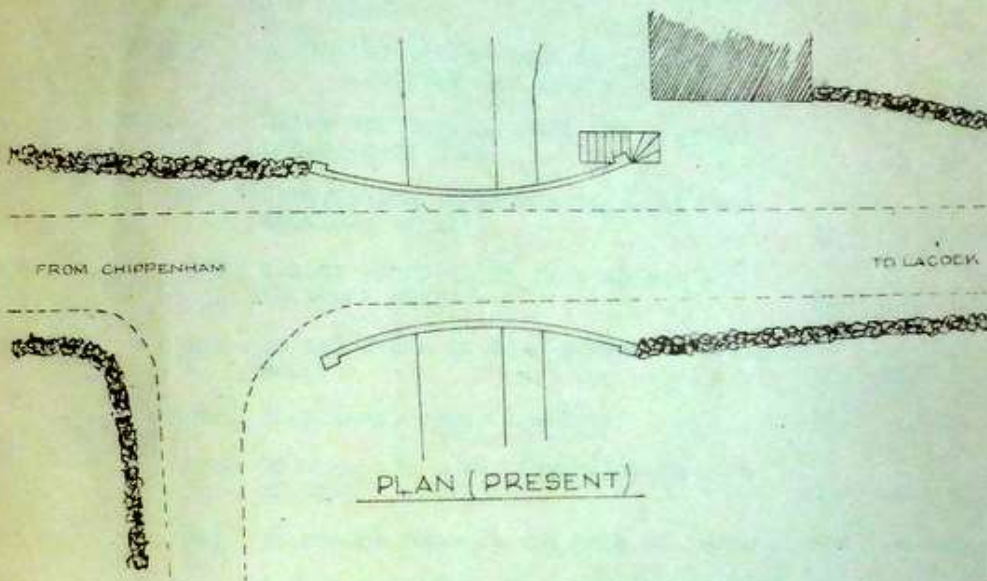
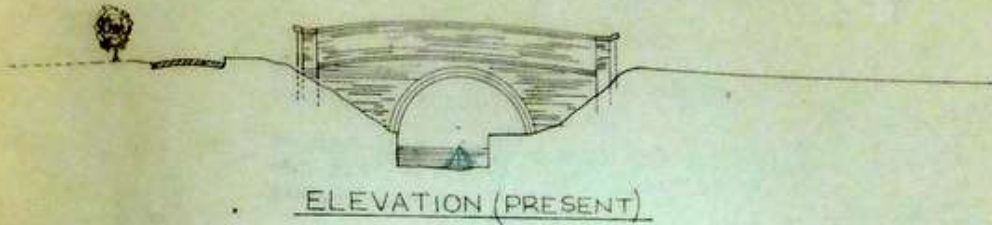
PLAN (PROPOSED)



# BOWDEN HILL BRIDGE.

CHIPPENHAM R.D.C.

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# CITY ROAD BRIDGE FOXHAM.

C. R. D. C.



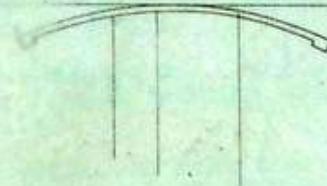
ELEVATION (PRESENT)



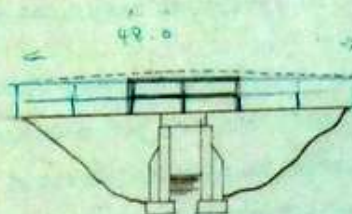
CHRISTIAN MALPOM

TO

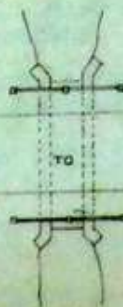
LYNEHAM



PLAN (PRESENT)



ELEVATION (PROPOSED)



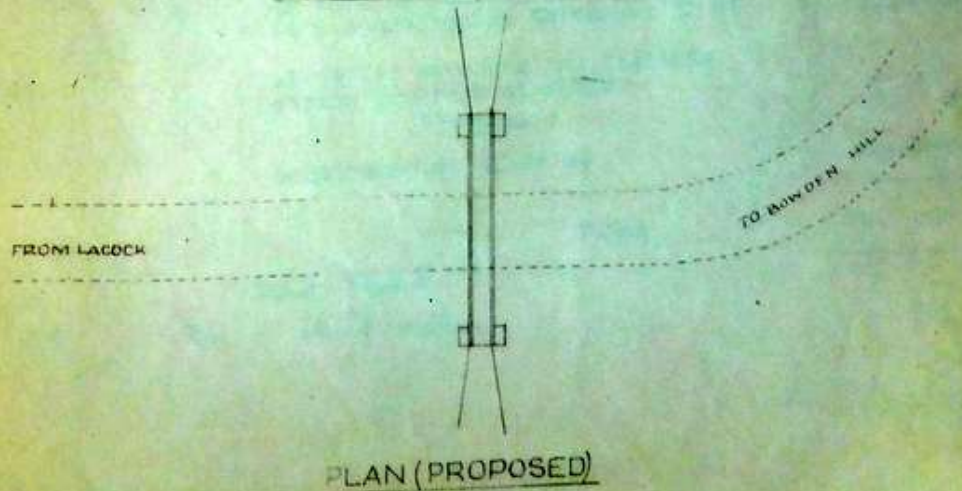
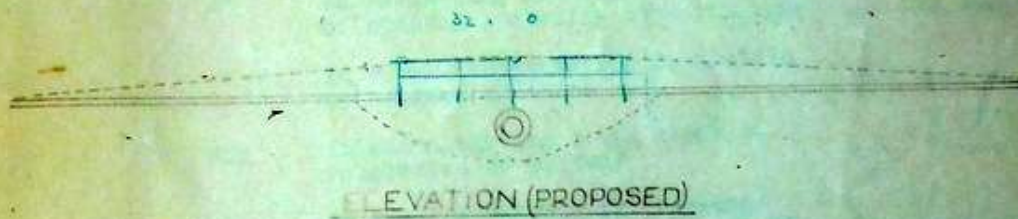
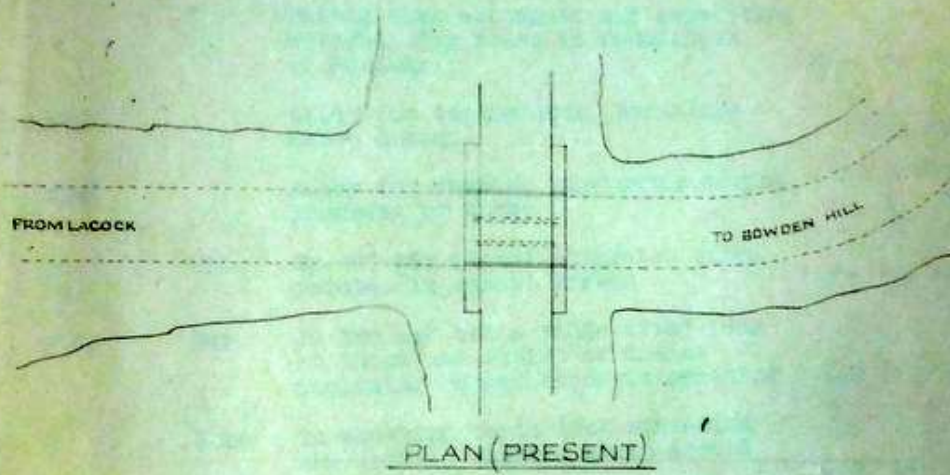
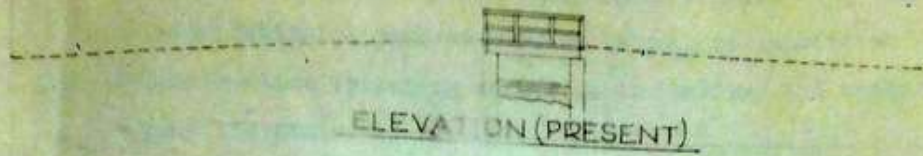
CHRISTIAN MALPOM

TO

LYNEHAM

PLAN (PROPOSED)

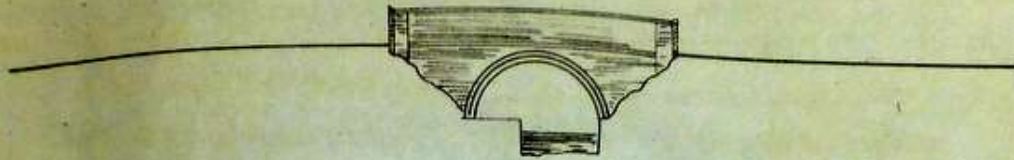
REY BRIDGE.  
CHIPPENHAM R.D.C.



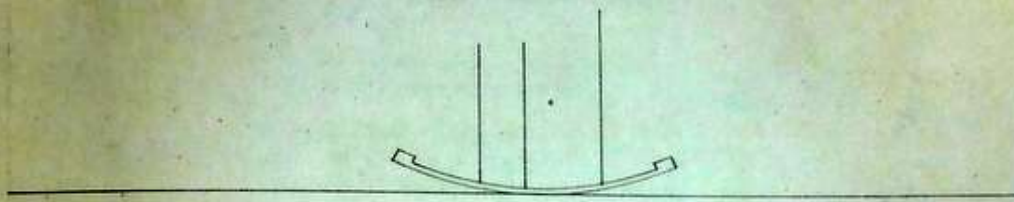


# STUDLEY BRIDGE.

CHIPPENHAM R.D.C.

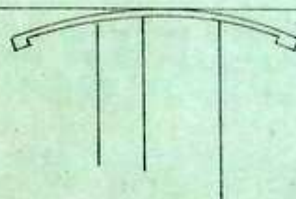


ELEVATION (PRESENT)

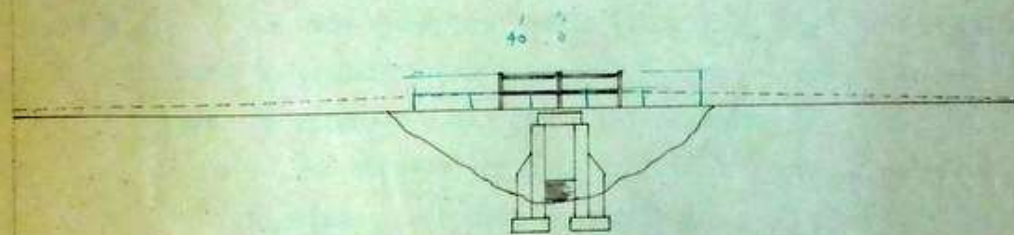


FROM CHIPPENHAM

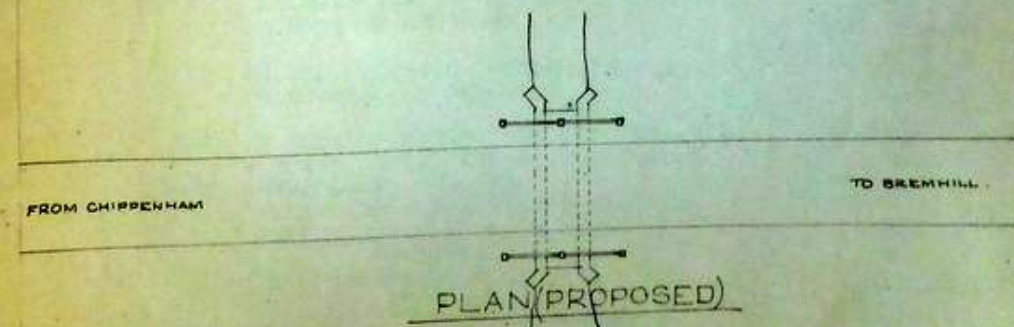
TO BREMHILL



PLAN (PRESENT)



ELEVATION (PROPOSED)



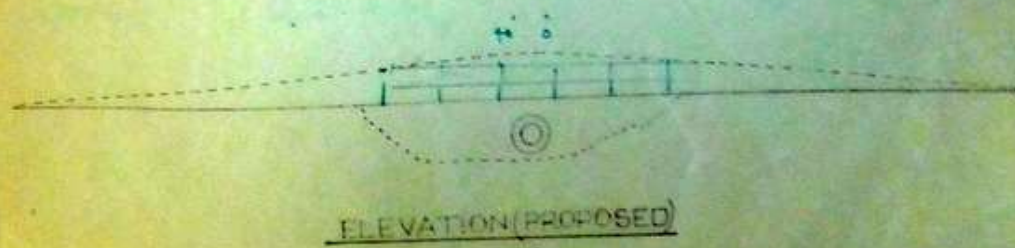
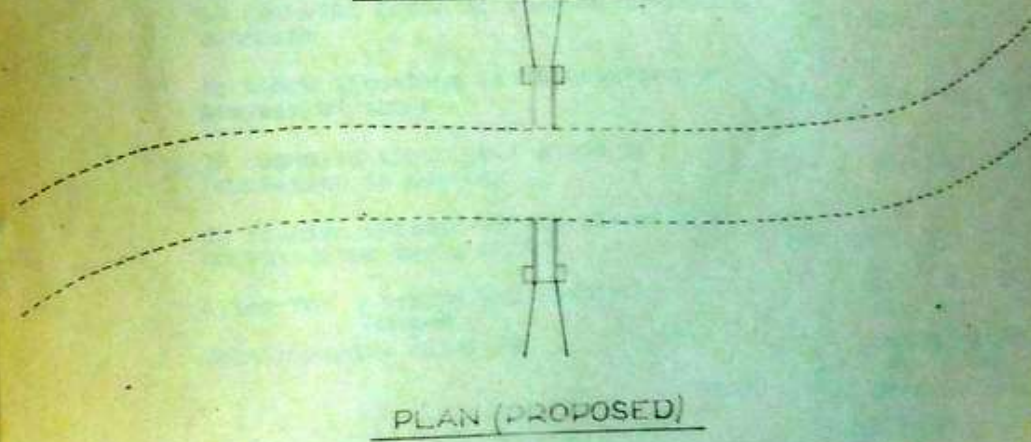
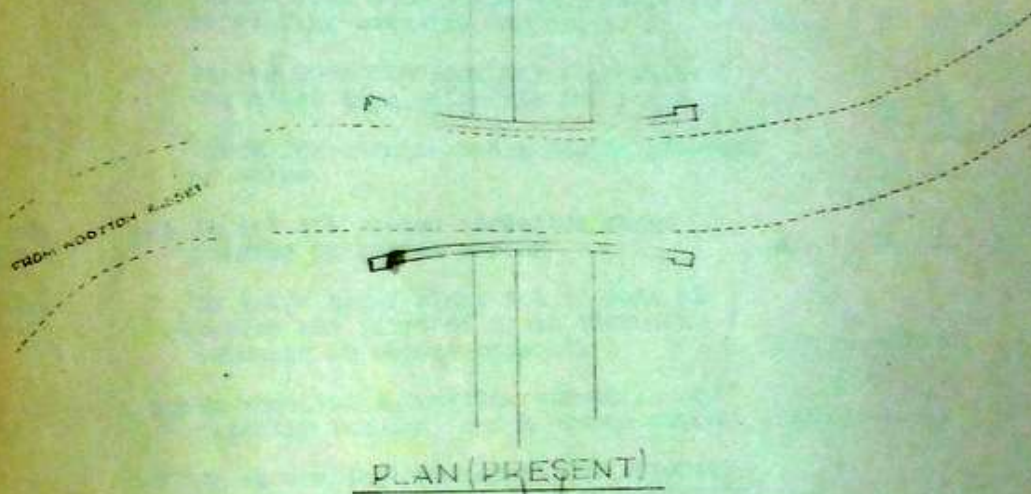
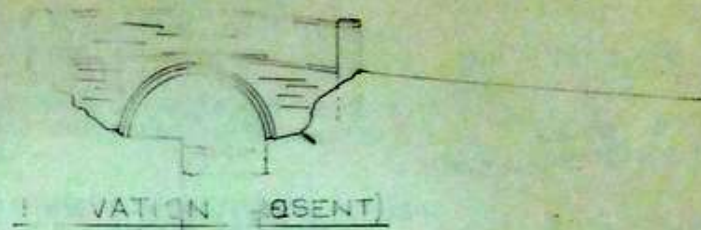
FROM CHIPPENHAM

TO BREMHILL

PLAN (PROPOSED)

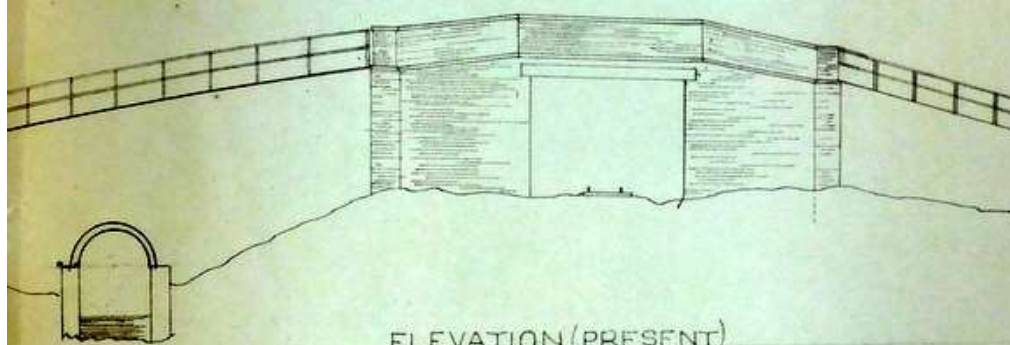
# BISHOP FOWLEY BRIDGE.

C. K. L. E. R. C.

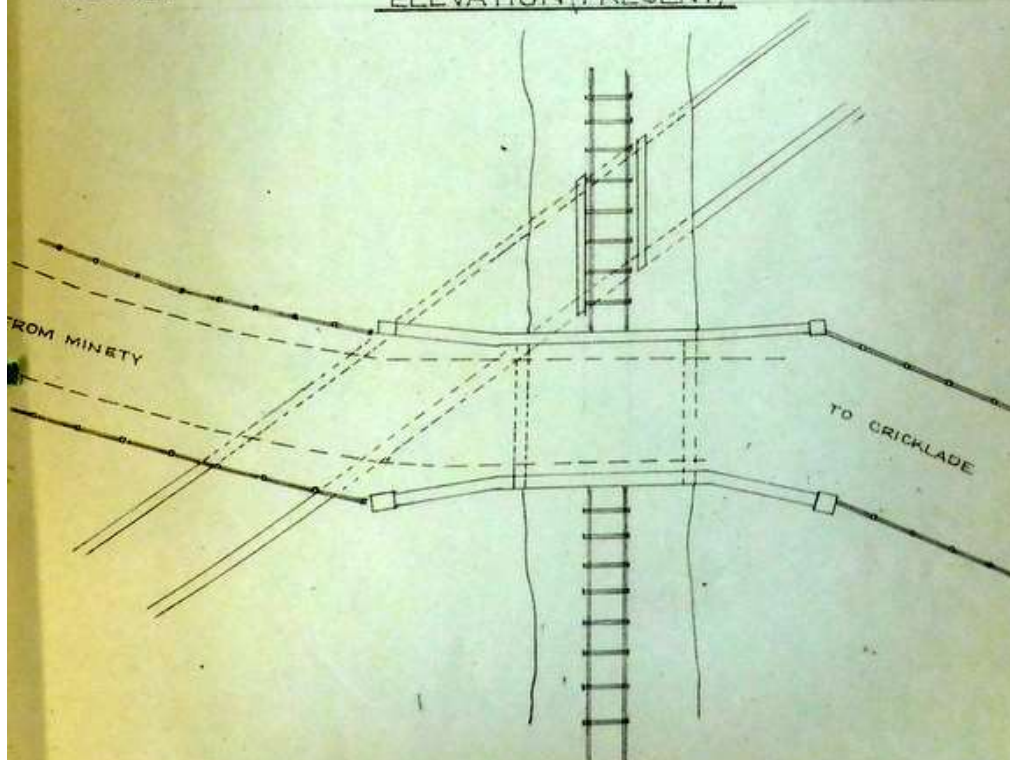




CANAL TUNNEL CRICKLADE.  
CRICKLADE R.D.C.



ELEVATION (PRESENT)

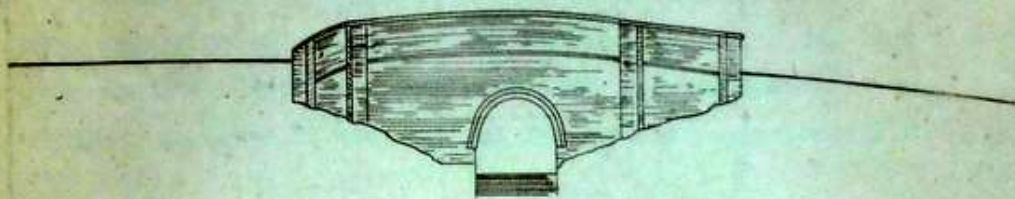


PLAN (PRESENT)

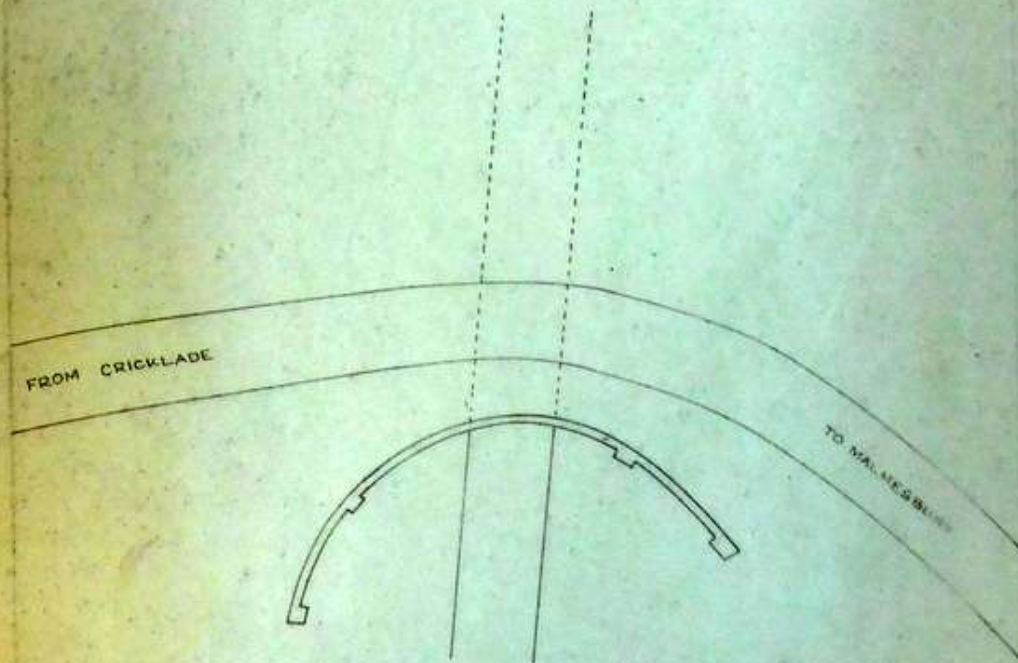
The layout was complicated because railway crossed over canal, then road bridge crossed over both.

# CRICKLADE TUNNEL BRIDGE

CRICKLADE R.D.C.



ELEVATION (PRESENT)



PLAN (PRESENT)



ORICKLADE TUNNEL BRIDGE.

The Brickwork in exposed walls is perished in many places. The defective bricks should be cut out and replaced by new. The mortar joints should be raked out and the joints repointed in cement mortar.

I estimate the cost of carrying out the work at approximately £60.

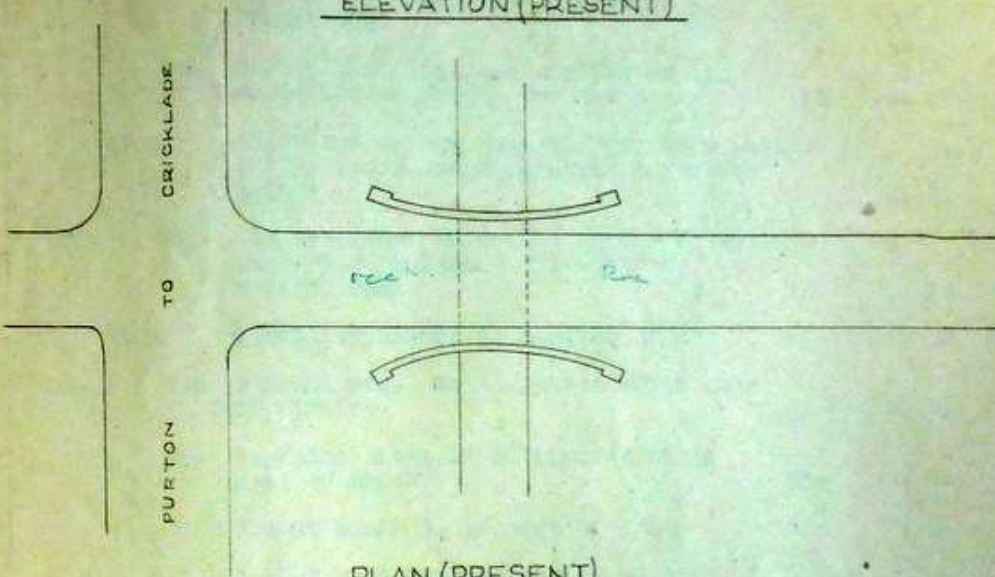
"The Brickwork in exposed walls is perished in many places. The defective bricks should be cut out and replaced by new. The mortar joints should be raked out and the joints repointed in cement mortar. I estimate the cost of carrying out the work at approximately £60."

# CROSS LANES BRIDGE.

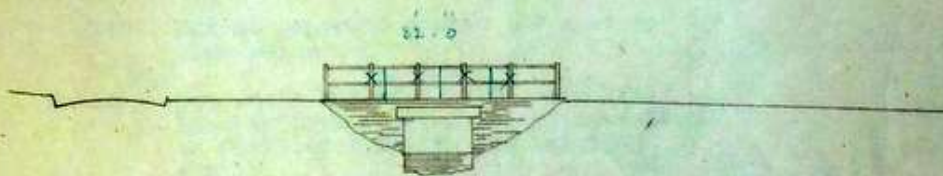
CRICKLADE R.D.C.



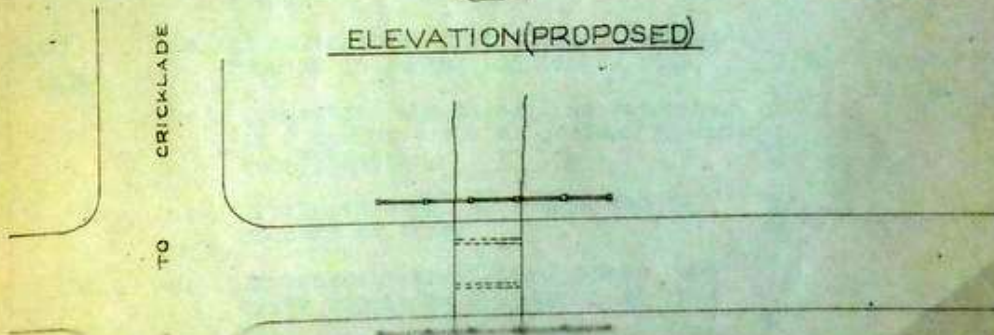
ELEVATION (PRESENT)



PLAN (PRESENT)



ELEVATION (PROPOSED)

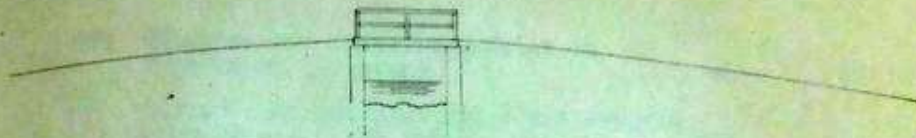


PLAN (PROPOSED)

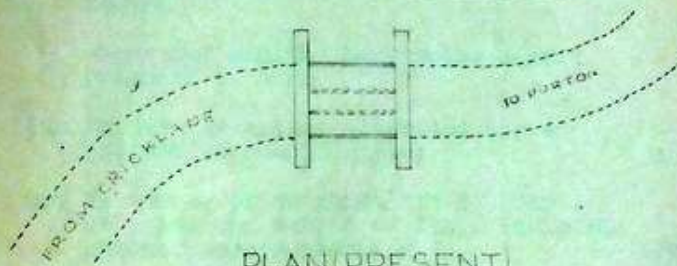


# DUDGE MOOR DRAW BRIDGE.

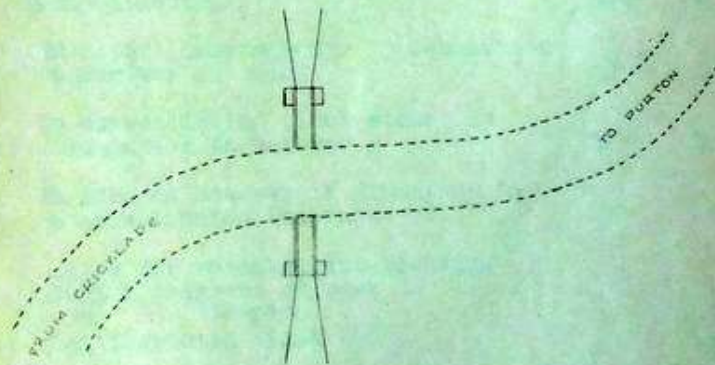
CRICKLADE R.D.C.



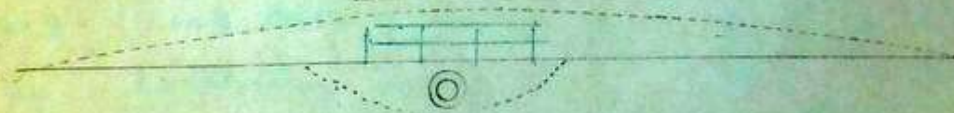
ELEVATION PRESENT



PLAN (PRESENT)



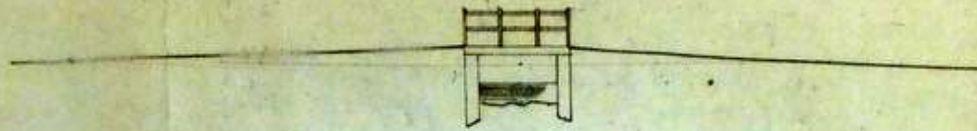
PLAN (PROPOSED)



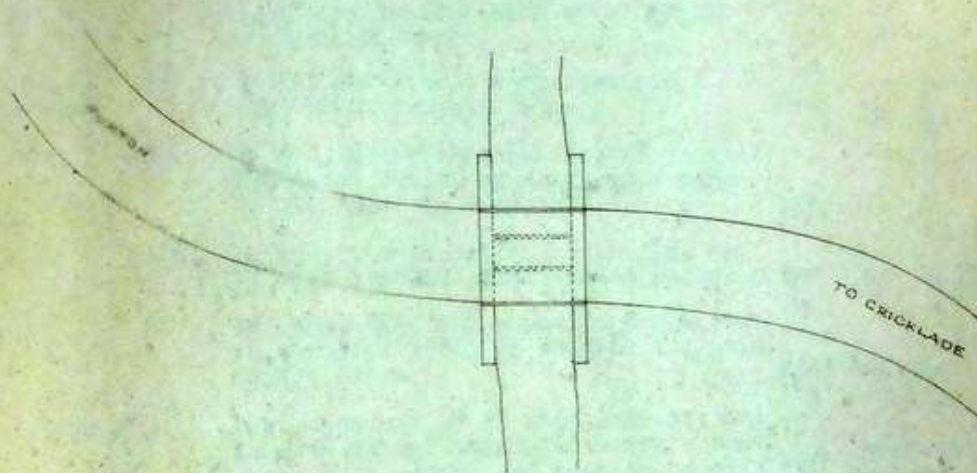
ON USED

# DUDGEMORE No2 DRAW BRIDGE.

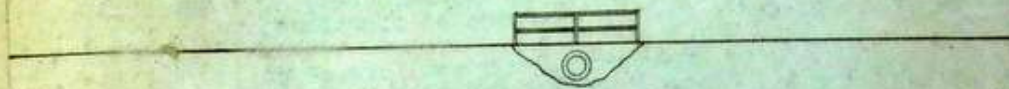
CRICKLADE R.D.C.



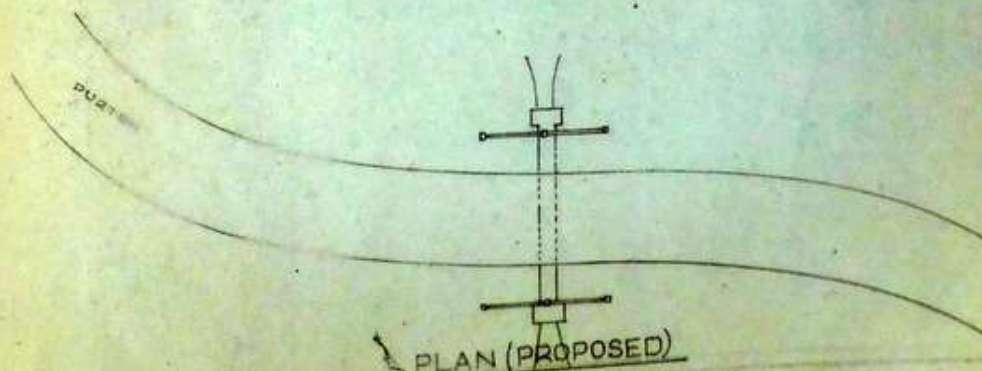
ELEVATION (PRESENT)



PLAN (PRESENT)



ELEVATION (PROPOSED)

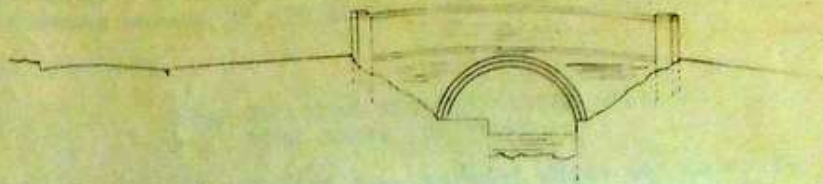


PLAN (PROPOSED)

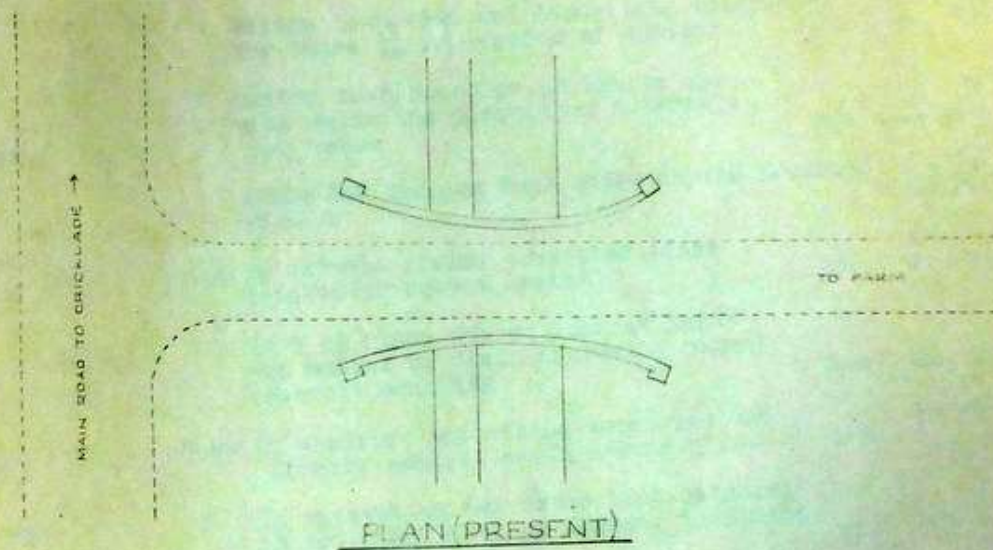


# DUDGE MOOR BRIDGE. No 3.

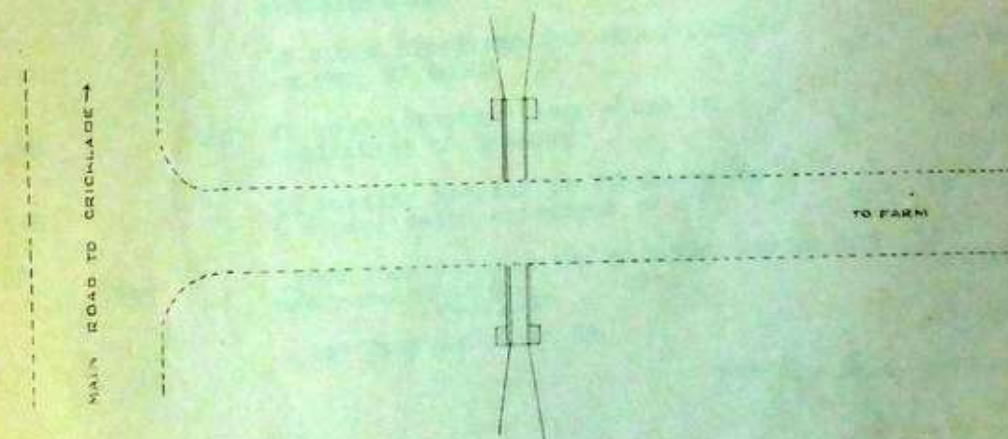
CRICKLADE R.D.C.



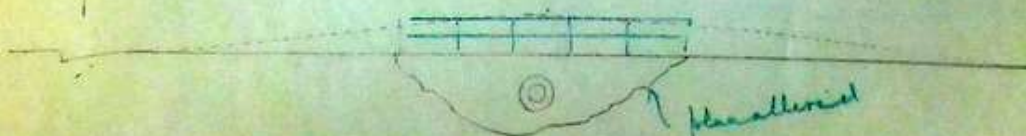
ELEVATION (PRESENT)



PLAN (PRESENT)



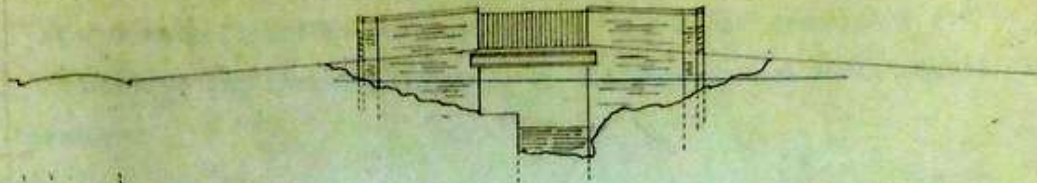
PLAN (PROPOSED)



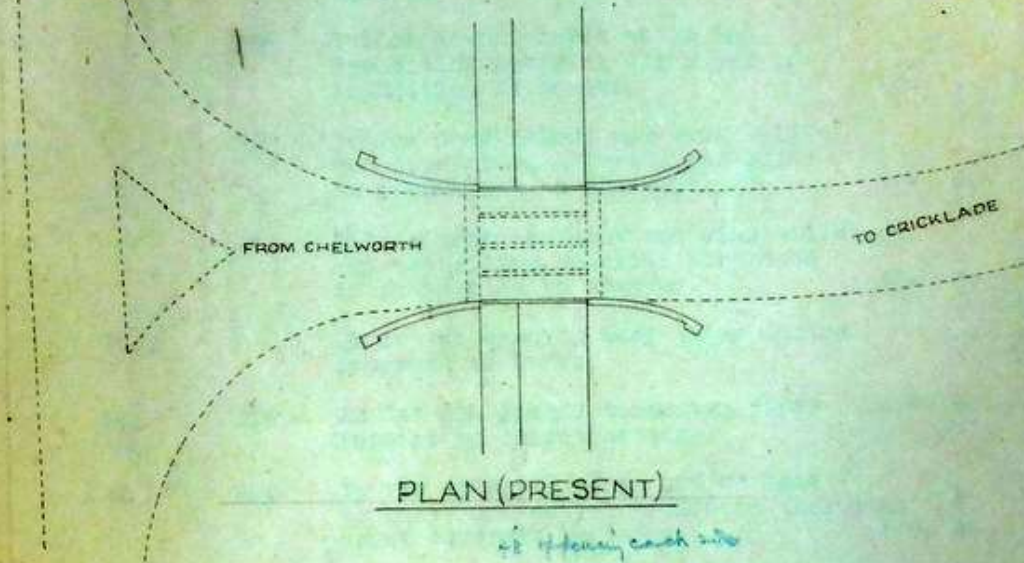
ELEVATION (PROPOSED)

# FORTY BRIDGE.

CRICKLADE R.D.C.



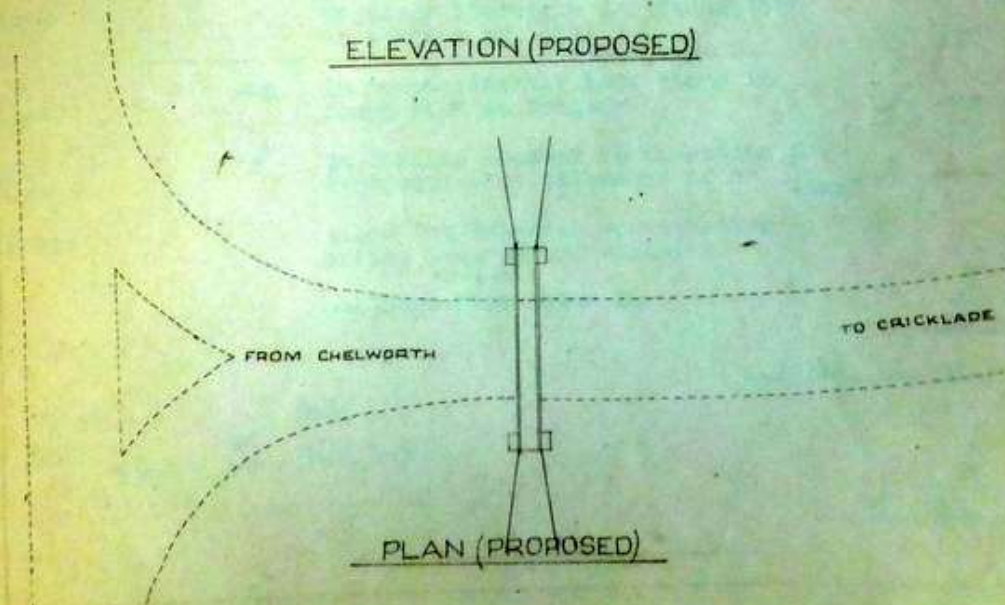
ELEVATION (PRESENT)



PLAN (PRESENT)



ELEVATION (PROPOSED)

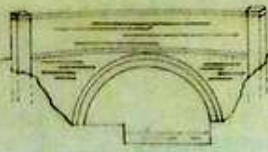


PLAN (PROPOSED)

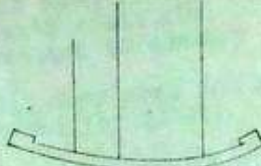


# GREENHILL BRIDGE No 1.

CRICKLADE R.D.C.

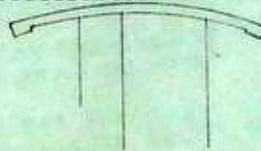


ELEVATION (PRESENT)



FROM WOOTTON-BAGSETT

TO TOCKENHAM



PLAN (PRESENT)



FROM WOOTTON-BAGSETT

TO TOCKENHAM



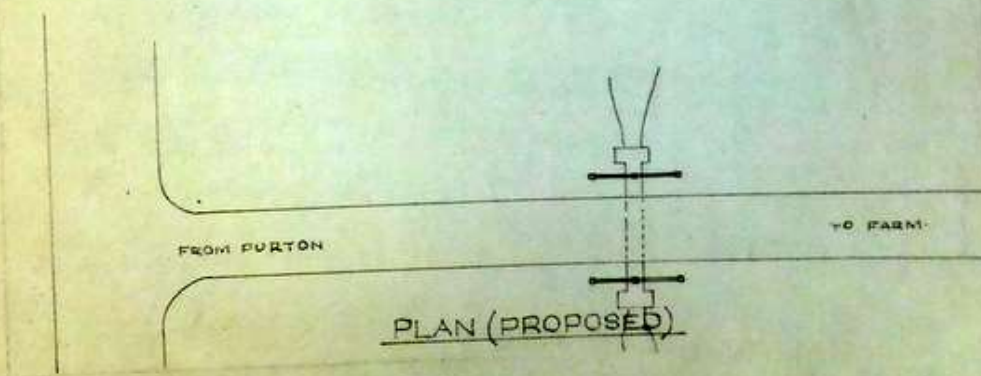
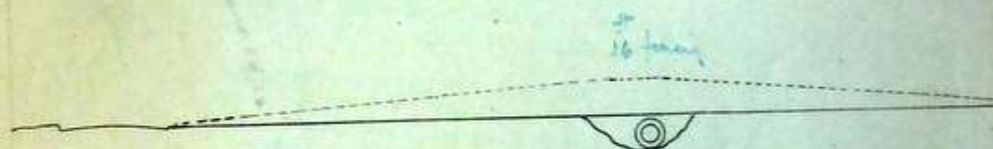
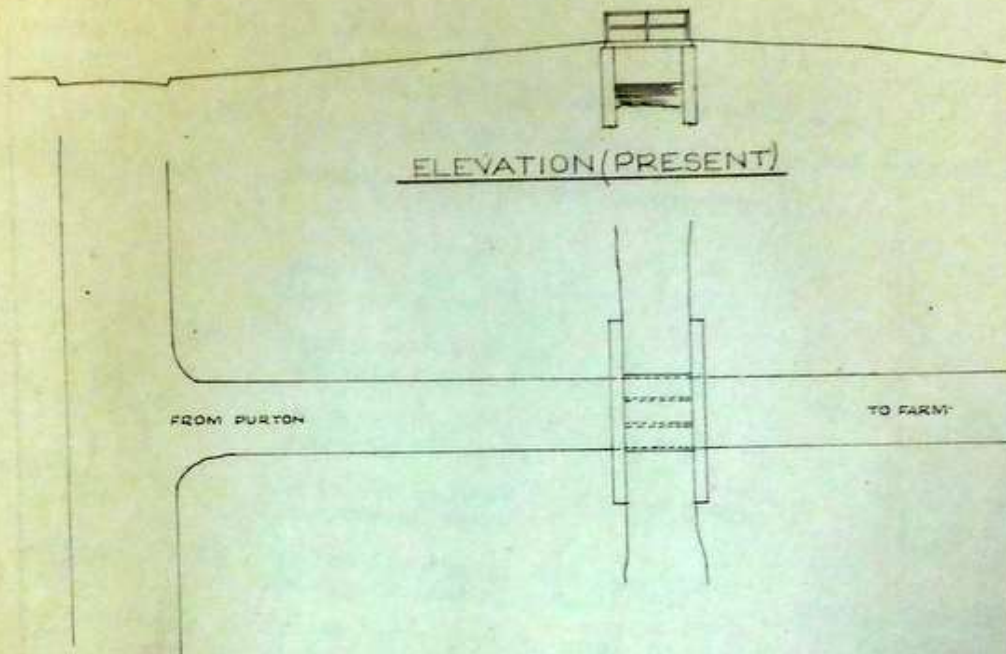
PLAN (PROPOSED)

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ATTN. PROPOSED

# PURTON PRY DRAW BRIDGE

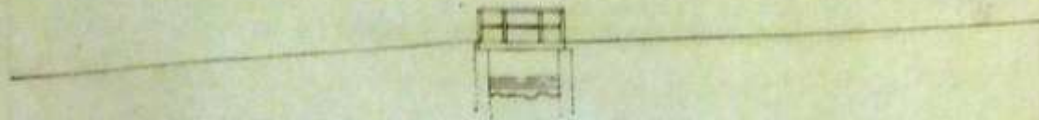
CRICKLADE R.D.C.



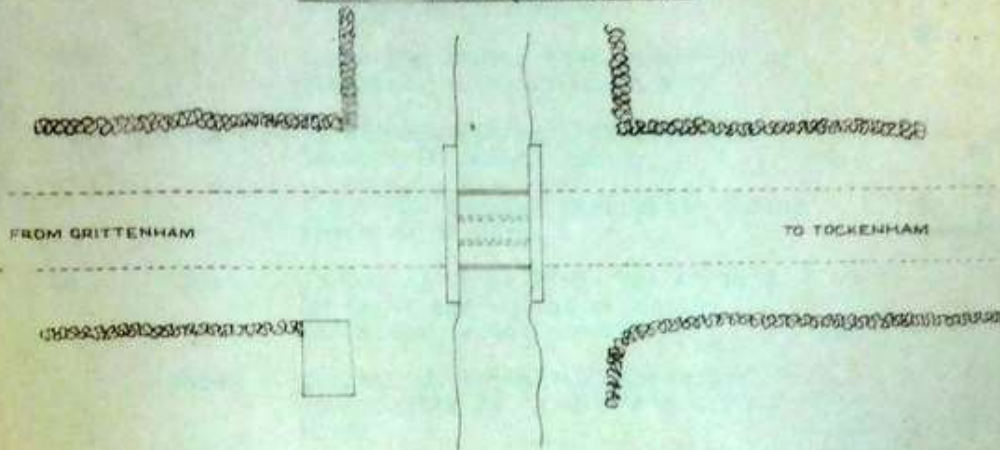


# TOCKENHAM WICK DRAW BRIDGE.

CRICKLADE R.D.C.



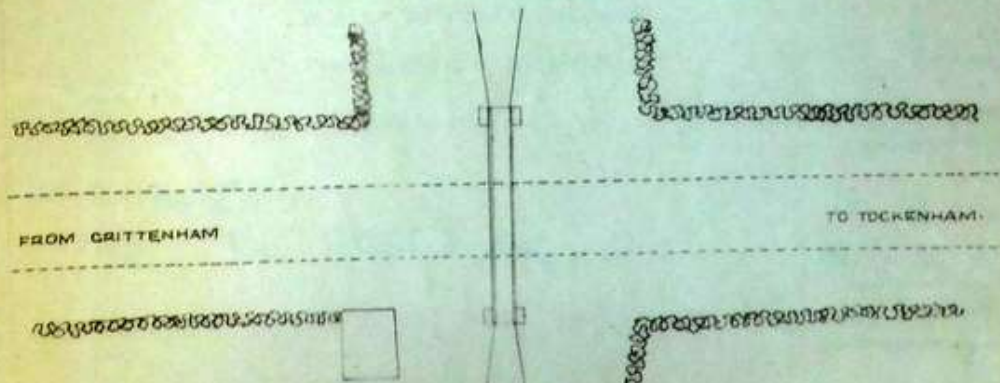
ELEVATION (PRESENT)



PLAN (PRESENT)



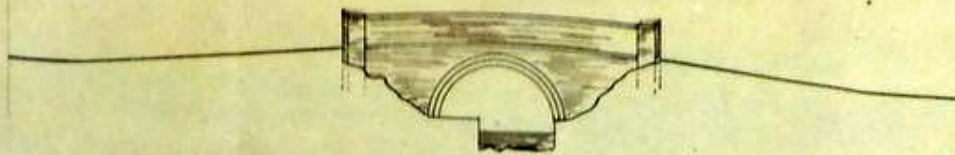
ELEVATION (PROPOSED)



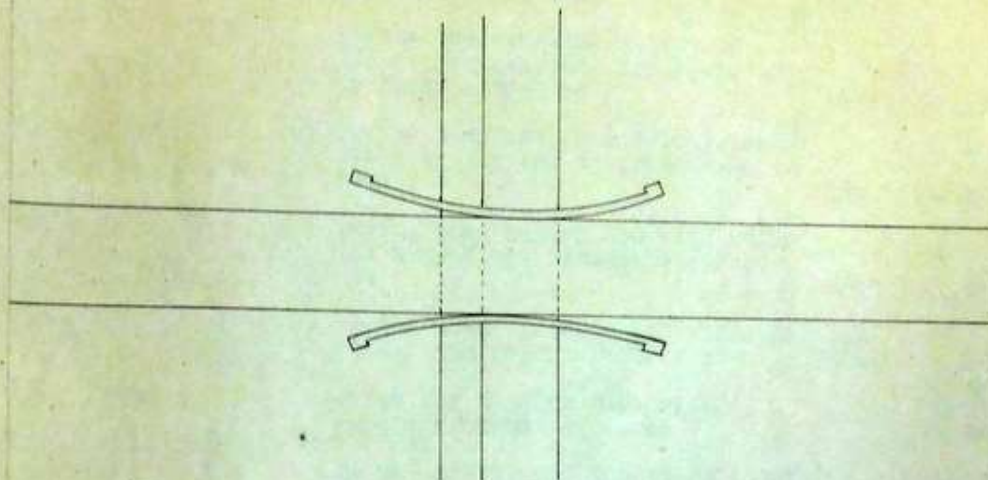
PLAN (PROPOSED)

# WOOD SHAW BRIDLE BRIDGE.

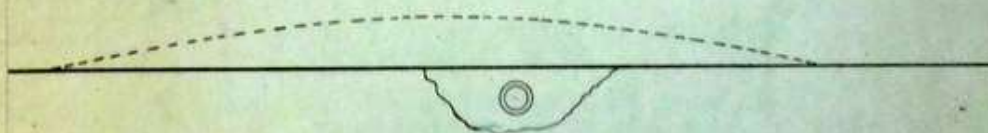
CRICKLADE R.D.C.



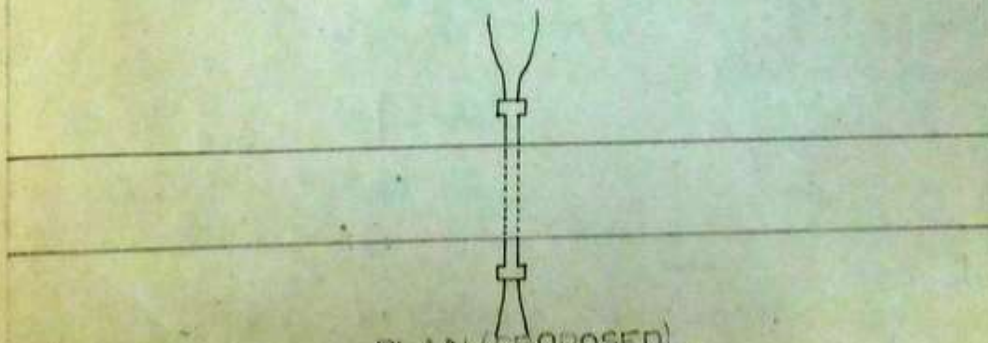
ELEVATION (PRESENT)



PLAN (PRESENT)



ELEVATION (PROPOSED)



PLAN (PROPOSED)

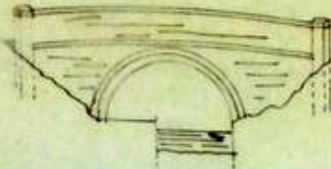


# BASSETT BRIDGE.

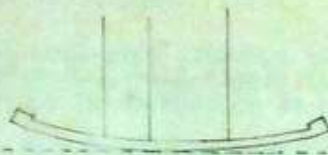
KILNORTH R.D.C.

*Cricklade*

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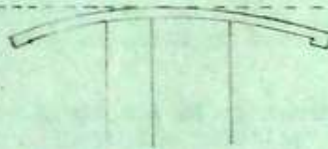


ELEVATION (PRESENT)

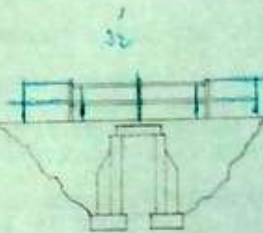


FROM LYDIARD TREGOZE

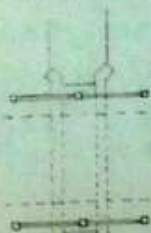
TO WROUGHTON



PLAN (PRESENT)



ELEVATION (PROPOSED)



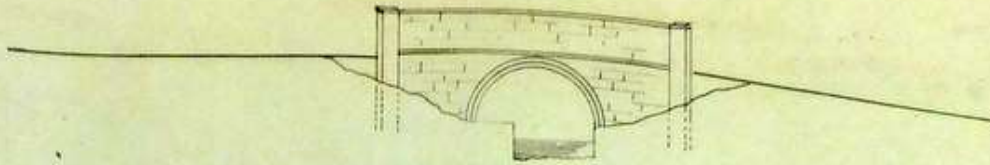
FROM LYDIARD TREGOZE

TO WROUGHTON

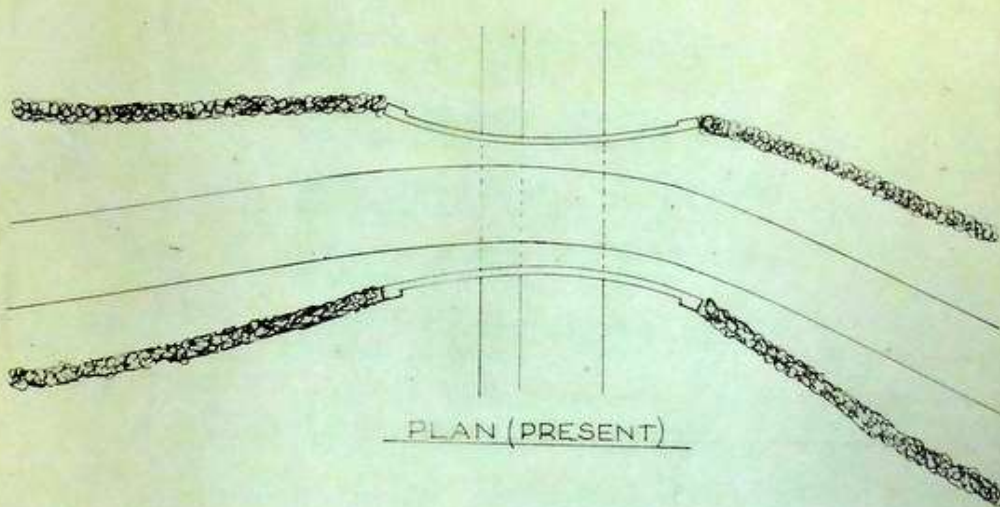
PLAN (PROPOSED)

# MOREDON BRIDGE.

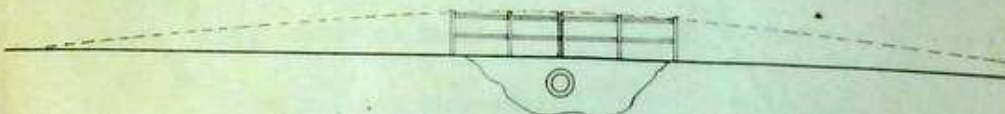
HIGHWORTH R.D.C.



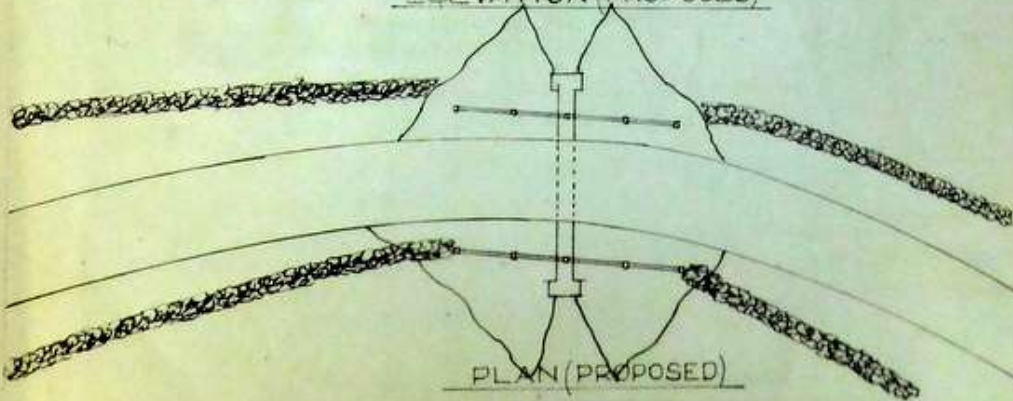
ELEVATION (PRESENT)



PLAN (PRESENT)



ELEVATION (PROPOSED)

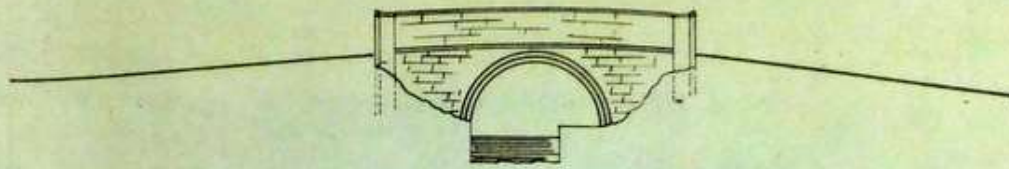


PLAN (PROPOSED)

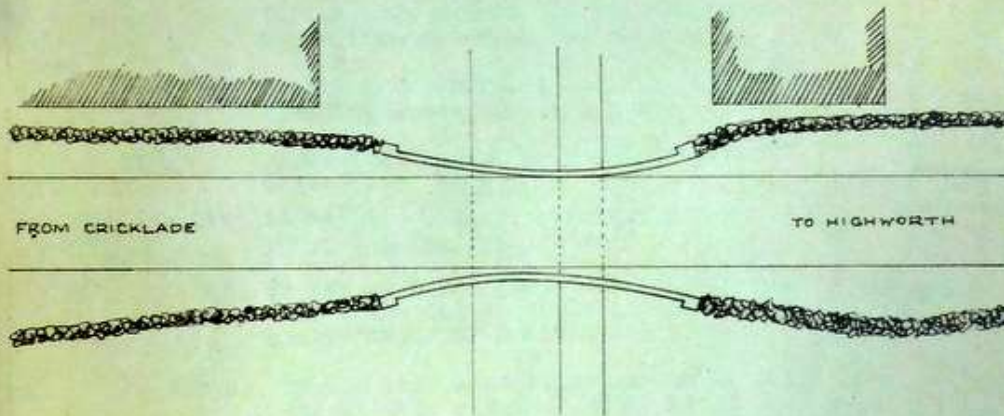


# STRATTON WHARF BRIDGE

HIGHWORTH R.D.C.



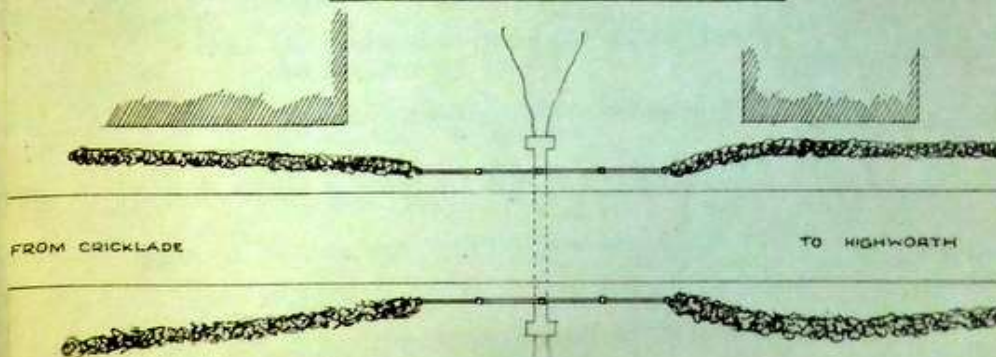
ELEVATION (PRESENT)



PLAN (PRESENT)



ELEVATION (PROPOSED)



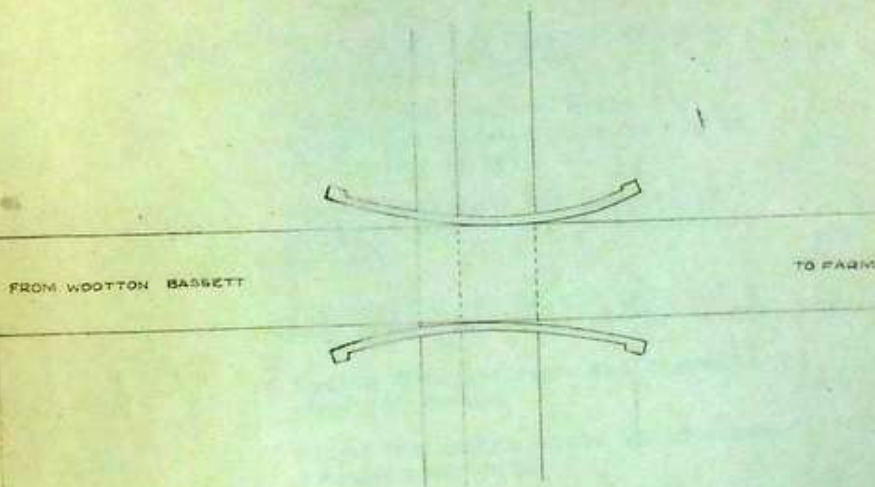
PLAN (PROPOSED)

# TOCKENHAM BERNARD BRIDLE BRIDGE.

*Chapworth*



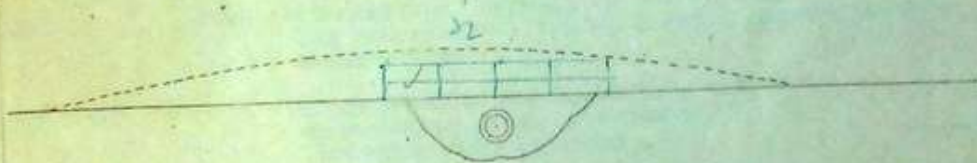
ELEVATION (PRESENT)



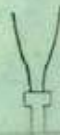
FROM WOOTTON BASSETT

TO FARM

PLAN (PRESENT)



ELEVATION (PROPOSED)

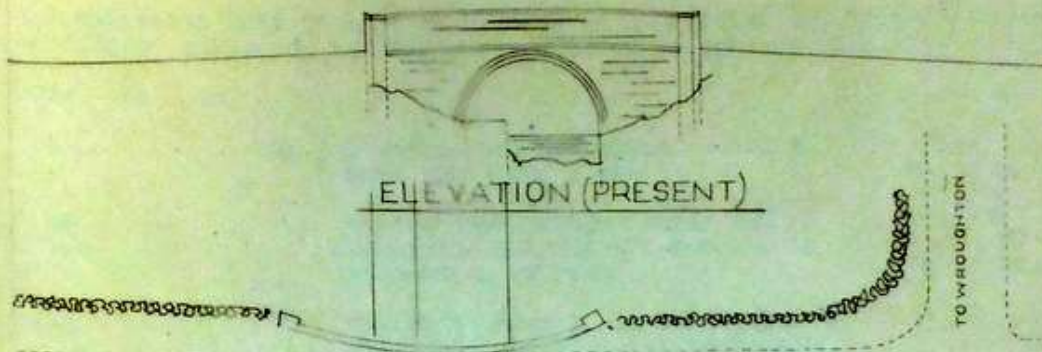


PLAN (PROPOSED)



# WROUGHTON BRIDGE.

HIGHWORTH R.D.C.



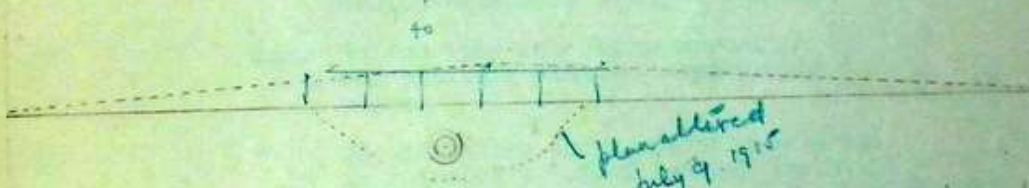
ELEVATION (PRESENT)

FROM WOOTTON BASSETT

TO WROUGHTON



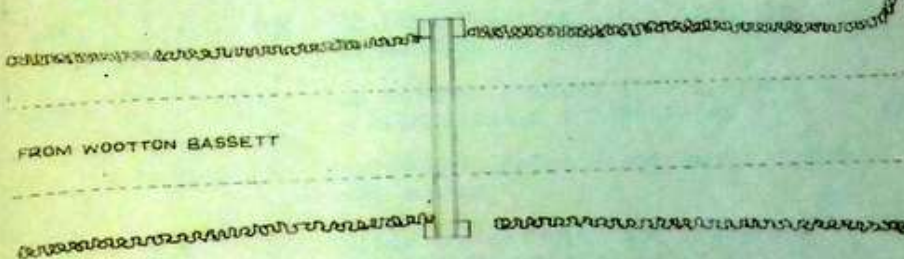
PLAN (PRESENT)



ELEVATION (PROPOSED)

FROM WOOTTON BASSETT

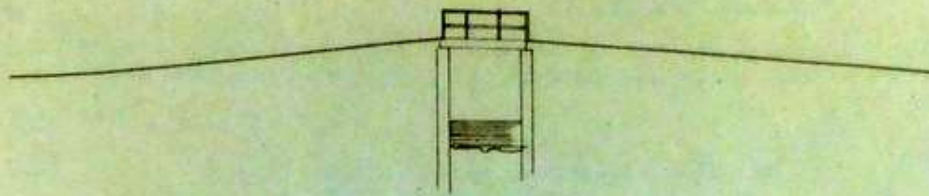
TO WROUGHTON



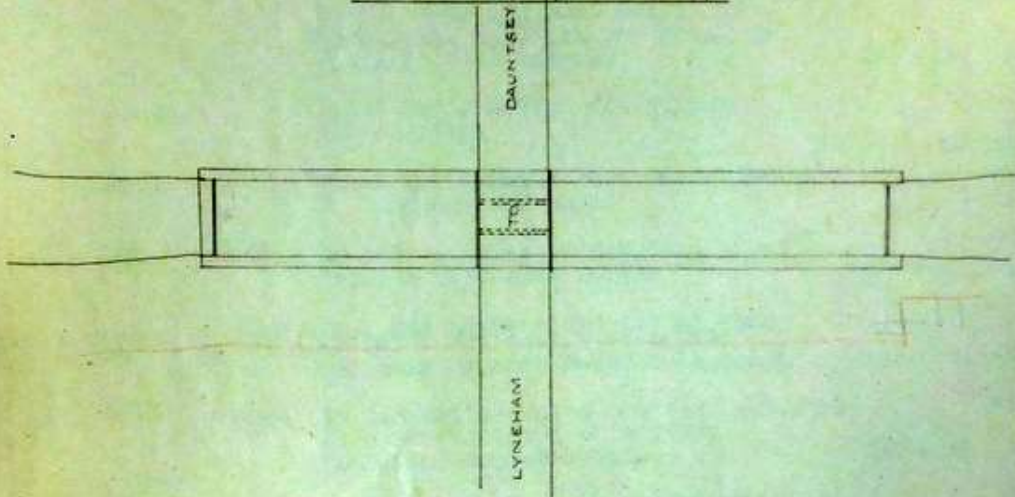
PLAN (PROPOSED)

# BOWD'S FARM BRIDGE.

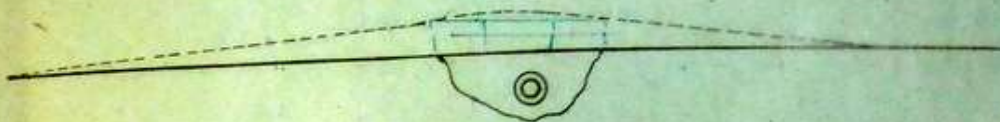
MALMESBURY R.D.C.



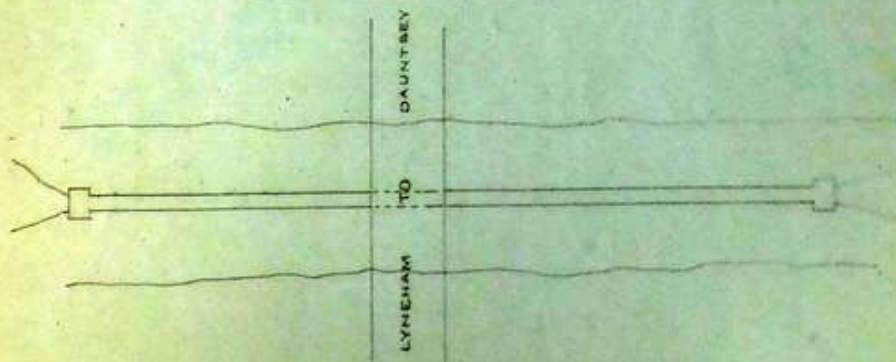
ELEVATION (PRESENT)



PLAN (PRESENT)



ELEVATION (PROPOSED)

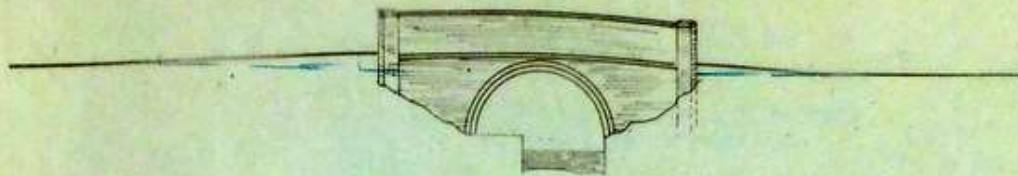


PLAN (PROPOSED)

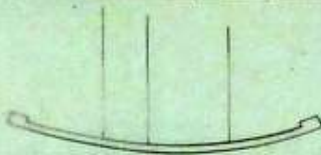


# TROW LANE BRIDGE.

MALMESBURY R.D.C.



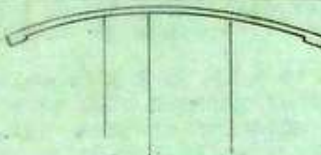
ELEVATION (PRESENT)



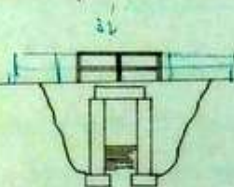
LYNEHAM

TO

DAUNTSEY



PLAN (PRESENT)



ELEVATION (PROPOSED)



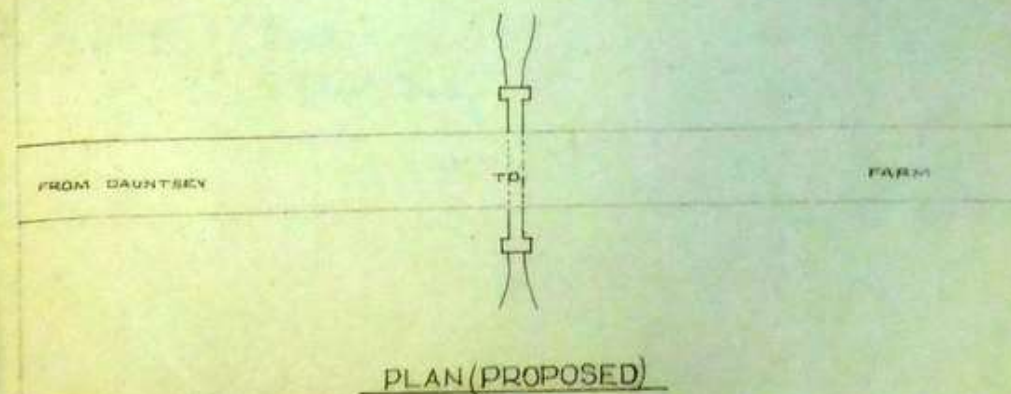
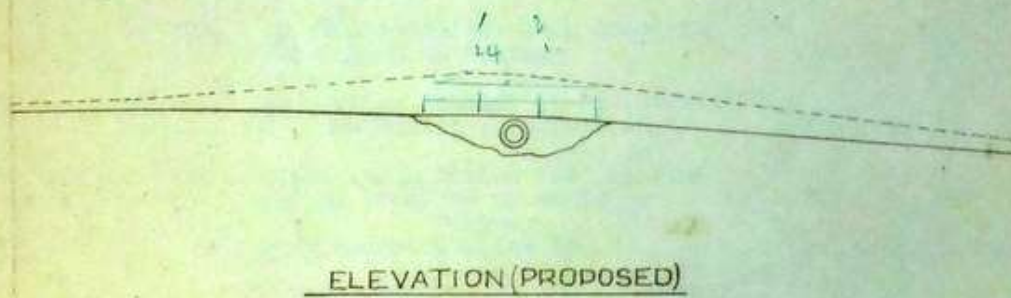
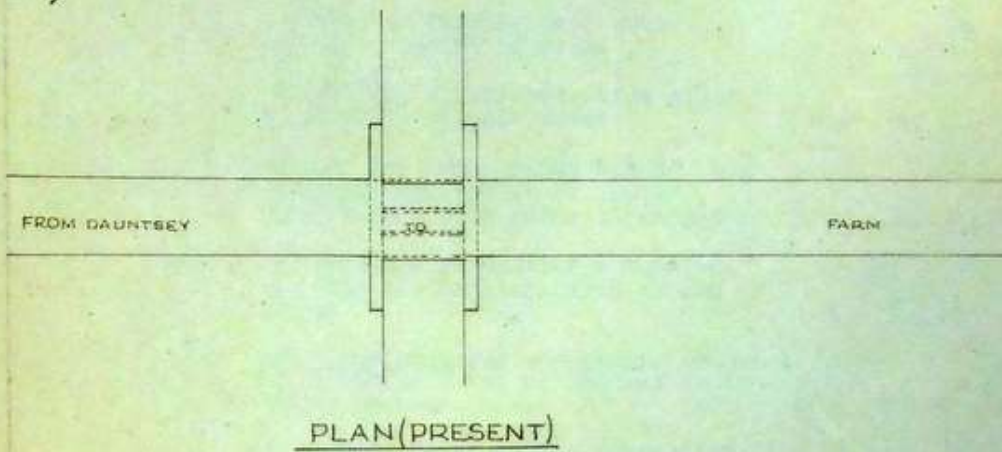
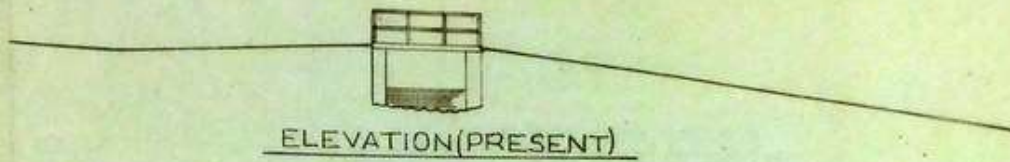
LYNEHAM

TO

DAUNTSEY

# WAITE HILL FARM DRAW BRIDGE.

MALMESBURY R.D.C.





## Summary Page

S U M M A R Y				£.	s.	d.
1781 10 2 (8)	Canal Bridges on County Main Roads			3126.	7.	<u>3.1788.14.2</u>
767 19 0 (7)	Ditto	Calne Rural District		626.	11.	<u>4.663.14.10</u>
493 12 10 (4)	Ditto	Chippenham Rural District		468.	0.	<u>0.419.6.0</u>
1170 10 11 (12)	Ditto	Cricklade Rural District		837.	3.	<u>6.838.11.7</u>
564 0 3 (5)	Ditto	Highworth Rural District		853.	16.	<u>9.621.4.6</u>
303 11 1 (3)	Ditto	Malmesbury Rural District		334.	14.	<u>0.240.10.7</u>
<u>5081 4 3</u>		Total		<u>4916.</u>	<u>13.</u>	<u>9.4572.1.8</u>